

# MINT HILL PEDESTRIAN MASTER PLAN



**Adopted by:**

Town of Mint Hill Board of Commissioners

**Adopted:**

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**Submitted by:**

**HNTB**

**In conjunction with:**



Donal Simpson



## **MINT HILL PEDESTRIAN MASTER PLAN**



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# ACKNOWLEDGEMENTS





## **MINT HILL PEDESTRIAN MASTER PLAN**



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## Steering Committee Members

Chris Cochrane

Tim Ledford

Jeff Freeman

Tom Gatz

Jay Brosnan

Charlie Burdick

Rolf Jaeger

Frank Burns

## Board of Commissioners

Ted H. Biggers, Mayor

Lloyd Austin, Mayor Pro-Tem

Carol M. Ellington

Brenda McRae

Katrina "Tina" Ross

## Town Staff

Lee Bailey, AICP

Dana Clukey

John Hoard

**North Carolina Department of Transportation**

Robert Mosher, ASLA, AICP

**Consultant (HNTB)**

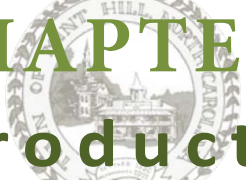
Kevin Walsh, LEED AP

Padam Singh, AICP, LEED AP

Paige Hunter, E.I.

**Consultant (Independent)**

Donal Simpson, AICP, FAIA



# CHAPTER 1

## Introduction



## MINT HILL PEDESTRIAN MASTER PLAN



# Introduction

## 1.1 Context

The Town of Mint Hill has seen its share of growth over the past 20 years due to its proximity to the City of Charlotte, its attractive rural and small town character, and its access to major retail and employment centers. In addition, Mint Hill is geographically located in an area that is one of the fastest growing areas in the United States.

According to the State of North Carolina Office of State Budget and Management, in 2000, the population of Mint Hill was 15,609. In comparison, the estimated population in 2009 was slightly above 21,000, an increase of almost 7,000<sup>1</sup>. In addition, there is a percentage of the population that does not have a driver's license that depends on other modes of transportation. According to the US Census Bureau, the total estimated population of North Carolina in 2008 was 9.3 million. Of that, approximately 6.5 million had a driver's license<sup>2</sup>; leaving roughly 30% of the state's population without one. Therefore, approximately 2.7 million people across North Carolina depend on other modes of transportation besides the automobile. Therefore, it can be assumed that there is a percentage of Mint Hill's population that does not have a driver's license and relies on other forms of transportation.

This signifies a growing concern among many jurisdictions, that the number of people who depend on alternative modes of transportation is large. This concern is even higher in growing suburban communities like the Town Mint Hill, where access to public transit is limited and recent development of residential subdivisions and commercial shopping centers are only connected via a network of rural thoroughfares.

## 1.2 Purpose of a Pedestrian Master Plan

In 2008, the Town of Mint Hill completed the development of its Comprehensive Transportation Plan (CTP). The CTP provided many recommendations for the transportation network, including a specific recommendation to complete a comprehensive pedestrian plan. The Pedestrian Master Plan is a direct result of that recommendation. The development of this plan comes at a very strategic time for the Town of Mint Hill. The Town is currently in the process of updating its Comprehensive Land Use Plan and planning for growth over the next 20 years. Understanding where growth will be directed over the next several years will assist the Town in focusing on where to invest in pedestrian infrastructure. Currently, there is a lack of pedestrian infrastructure, especially at the major intersections throughout the town.

**"Additionally, this plan recommends that the Town complete comprehensive bicycle and pedestrian plans through the NCDOT Division of Bicycle and Pedestrian Transportation."**

Mint Hill Comprehensive Transportation Plan

## 1.3 Benefits of Walking

### 1.3.1 Overview

Communities across America have become more aware of the need to provide multi-modal connectivity and to develop “complete streets” where all users are accounted for. There are many benefits that can be realized through increased walking and physical activity. There are individual benefits, such as increased health and well being, but there are other benefits that have an effect on those around us. The following are types of benefits that can come from increased walking and physical activity.

- Health and Physical Activity
- Transportation
- Environmental
- Economic
- Quality of Life

### 1.3.2 Health Benefits

The issue of physical activity has never been more important than it is today. Research by the Center for Disease Control has shown that an alarming number of Americans are becoming more inactive and obese and, consequently, are putting their lives at risk. In 2007-2008, the prevalence of obesity was 32% among adult men and 36% among adult women.<sup>3</sup> Engaging in light to moderate physical activity reduces the risk of heart disease, stroke, and other chronic illnesses. Physical activity has also shown to improve an individual’s mental health and can even lower health care costs.<sup>4</sup>

Therefore, developing a comprehensive and multi-modal transportation network, which includes sidewalks and greenways, within Mint Hill will help increase the overall health of the community and will provide individuals with alternative transportation choices other than the automobile. For more information regarding the health benefits of walking, please go to the U.S. Department of Transportation Federal Highway Administration’s web page at: [http://www.fhwa.dot.gov/environment/bikeped/benefits\\_research.htm](http://www.fhwa.dot.gov/environment/bikeped/benefits_research.htm)



### 1.3.3 Transportation Benefits

Walking is by far the least expensive and most accessible of all forms of transportation. In fact, 40% of the trips that Americans make each and every



day are 2-miles or less in distance, which is equivalent to a 30 minute walk. Infrastructure improvements, such as the addition of landscaped medians, pedestrian lighting, wide sidewalks, crosswalks and pedestrian signals can increase pedestrian safety, as well as create a sense of place and promote social interaction by encouraging citizens to walk more. By improving the pedestrian network and providing sufficient infrastructure, citizens will be able to walk to their destinations safely and rely less on automobiles.

### **1.3.4 Environmental/Energy Benefits**

Many environmental benefits can be realized through the expansion of sidewalks and the addition of greenways in Mint Hill. Many of the greenways that are shown in this plan utilize creek and stream corridors. According to the National Trails Training Partnership, greenways improve water quality by creating a natural buffer that protects streams and rivers. They also help improve air quality by providing safe and enjoyable transportation options.<sup>5</sup> They also protect local wildlife, such as the Carolina Heelsplitter, and encourage community interaction with nature.



***The Carolina Heelsplitter is an endangered species and needs to be protected***

The Environmental Protection Agency (EPA) estimates that motor vehicles are responsible for nearly 80 percent of carbon monoxide and 55 percent of nitrogen oxide emissions in the U.S. In fact, shorter car trips create more pollutants on a per-mile basis than longer trips because 60% of pollution created by automobile emissions occurs within the first few minutes of operation<sup>6</sup>. Therefore, providing transportation options can assist in improving air quality and help preserve valuable, non-renewable resources. Also, improved air quality can add to an individual's health benefits.

### **1.3.5 Economic Benefits**

Good pedestrian connectivity can have a real impact on the local economy for the Town of Mint Hill. First and foremost, sidewalks and greenways are less expensive to install and maintain than roadways. Also, according to the Pedestrian and Bicycle Information Center (PBIC), the level of walking is considered in many communities to be an indicator of a community's livability, a factor that has a profound impact on attracting businesses and workers as well as tourism. In cities and towns where people can regularly be seen out walking, there is a palpable sense that these are safe and friendly places to live and visit.<sup>7</sup>

From a real estate perspective, trails and greenways are considered important amenities that communities want incorporated into their subdivisions. In fact, a 2002 survey by the National Association of Home Builders and the National

Association of Realtors showed that walking/jogging trails ranked second out of a choice of 18 community amenities.<sup>8</sup>

### **1.3.6 Quality of Life**

The quality of life benefits that are realized through a comprehensive pedestrian network, though intangible, can have significant impacts on a communities overall wellness. As mentioned earlier, the perceived level of walkability throughout a town often indicates a community's livability to potential residents and businesses.

The social interaction possible when the number of people walking increases is a major factor in improving the quality of life for a community. Comfortable and accessible pedestrian environments offer alternatives to personal vehicles, which limit opportunities for social contact with others. By providing appropriate pedestrian facilities and amenities, communities enable the interaction between neighbors and other citizens that can strengthen relationships and contribute to a healthy sense of identity and place.<sup>9</sup> Neighborhoods like Brighton Park provide amenities such as greenways which are integrated into the fabric of the community.



*Greenways and Trails are some of the most desirable amenities in communities*

## **1.4 Public Outreach**

An extensive public outreach process was developed for the Pedestrian Master Plan in order to obtain input from the community. The public outreach process utilized traditional and non-traditional means to collect this information. The information gathered through each of these sources provided valuable insight into the needs of the community, the walking behaviors that currently exist and the important destinations that individuals want to be able to access in the future. In the future, the Town should constantly review the information gathered throughout this process to ensure that the priorities are being addressed. The list below represents the various techniques used to gain input throughout the process:

- Stakeholder Interviews
- Walking Audit with the Steering Committee
- Public Workshops
- Online Pedestrian Questionnaire (Survey Monkey)
- Facebook Group

### **1.4.1 Stakeholder Interviews**

In order to gain a better understanding of the issues facing the Town, a series of Stakeholder Interviews were conducted on July 28, 2010. Three different groups were interviewed throughout the day. These groups were comprised of individuals from the following groups:

- Town Staff, including Police, Public Works and Planning
- Homeowner Association Representatives
- Residents
- Charlotte-Mecklenburg Schools

The interview process was very open, allowing individuals to provide input on pedestrian related issues and concerns. Participants were asked to provide input on priority areas within the community and to list their main concerns and issues. Based on the feedback from each of the interviews, the following list of issues and concerns was created. This list represents the most common themes from those interviews and is not meant to capture everything that was mentioned. The information gathered from the interviews provided a foundation for the development of a Vision and a list of Goals that will be discussed later in this chapter.

#### Key Issues and Concerns from the Interviews

The following are some of the key issues and concerns that were voiced during the stakeholder interviews.

- Speed limits, especially in residential subdivisions and around schools are excessive.
- There is a lack of pedestrian crossing amenities at signalized intersections.
- Maintenance of sidewalks is a real issue for the Town's Public Works Department
- There are many schools within the Town of Mint Hill and more pedestrian amenities need to be provided to allow children to walk to them.
- Connectivity between future greenways and sidewalks will be important to make the town a more walkable place.
- There are gaps in the current sidewalk system, which discourages residents to walk to their destinations.

### **1.4.1 Public Workshops**

The first public workshop was conducted on August 31, 2010 at the Town's Municipal Building. The workshop provided residents with an opportunity to hear what the main issues and concerns were from the stakeholder interviews, understand the recommendations from previous planning efforts, and review and comment on the draft Pedestrian System Map. The meeting began with a presentation that provided an overview of the following:

- An explanation of the purpose of a pedestrian plan
- A review of the draft vision and goals
- An overview of the recommendations from the previous and on-going planning efforts
- Potential pedestrian improvement projects

Once the presentation was over, participants were asked to divide into groups and discuss the Draft Pedestrian System Map. Comments were recorded on the maps and on writing pads so that a comprehensive list of issues, concerns and priorities could be evaluated after the meeting. Additional projects not previously listed were also documented.

The second public workshop was facilitated on December 7, 2010 and was also located at the Town's Municipal Building. The purpose of this meeting was to allow citizens the opportunity to make final comments on the Pedestrian System Plan Map and review the project rating criteria and the top rated projects from the list of pedestrian projects. There was a short presentation that provided an overview of the proposed pedestrian projects from the System Plan Map, the rating criteria that was used to prioritize the projects and finally a review of the top rated project based on that criteria. Each participant was given a handout that contained the top rated projects for each category; sidewalks, greenways and intersection. Each participant was then asked to rank the top rated projects. This exercise provided information that allows the Town to better understand the community's priorities.

During the public meeting there was some concern regarding the proposed greenway alignments that were being illustrated on the proposed System Plan Map. More specifically, many people wanted to know when the project would be built as well as how it would affect their property. The citizens were informed by the consultants that the greenways are just conceptual lines on a



*Issue and concerns being discuss during the first public workshop*

map and are subject to change. In addition, the consultants mentioned that greenways take years to fund, plan and design before they become a reality. Funding for future planning and design have not been allocated.

#### ***1.4.2 Pedestrian Questionnaire***

In order to understand the needs of the community, a short questionnaire was developed for the Pedestrian Master Plan process and posted online using the web page Survey Monkey. The questionnaire asked general questions about where the user lived, how much they walk, where they would walk if there were sufficient pedestrian amenities, etc. A total of 124 respondents took the survey, and the information gathered from the questionnaire provided valuable insight into the community's priorities and walking habits. For instance, out of the 124 people that took the questionnaire, 108 (91%) of them said that they walk for exercise. Also, 103 (90%) of the respondents said that they would walk to parks or greenways if it were easy to walk to them.

Understanding the walking habits and destinations that are important to the community assisted in the development of pedestrian projects. These projects will be incorporated into the final plan and prioritized for implementation. To see the results from the survey, please see Appendix A.

#### ***1.4.3 Walking Audit with Steering Committee***

In order for the Steering Committee and the Town staff to truly understand the difficulties facing the pedestrian community, a walking audit was conducted on July 28, 2010. The walking audit route began at the Municipal Building located on NC 51 (Matthews-Mint Hill Road). The group started walking north along NC 51 toward Bain School Road discussing sidewalk requirements in the Town's Unified Development Ordinance (UDO). Currently the Town requires a minimum of 8-foot sidewalks within the Downtown Overlay District. One of the most recent examples of this requirement is the new development, Mint Hill Village, home to the future Town Hall. After this mixed use center, the group decided to cross NC 51 because the sidewalk ended. Once everyone crossed NC 51, the group continued toward Bain School Road. At Bain School Road, the group discussed the concerns that have been expressed by the community regarding the unsafe crossing conditions at this intersection. Many pedestrians, especially children, cross NC 51 at Bain School Road to access the Mint Hill Historical Village. There seemed to be an agreement amongst the



***Walking Audit participants walking on an 8-foot wide sidewalk in the Downtown Overlay District***



participants that improving the crossing conditions for this intersection should be a priority for the Town.

The group continued walking north along NC 51 toward the intersection of NC 218 (Wilgrove-Mint Hill Road). The group briefly stopped and discussed the importance of providing painted crosswalk and push-button signals for pedestrians at signalized intersections. Currently there is only one intersection that contains pedestrian amenities, such crosswalks and push-button signals. This intersection is known as the “Square” or NC 51 and Lawyers Road. The group continued the walking audit by walking north along NC 218 toward Nelson Road. The group turned onto Nelson Road and continued toward Brighton Park. Once in Brighton Park, the participants talked about the importance of working with developers to make good pedestrian connections from the residential areas to the commercial/retails areas. Finally, the group turned onto NC 51, completing the loop route. As the group approached the “Square” some of the participants talked about the short gaps in the sidewalks located in downtown. They mentioned that many residents in the community would like to see the sidewalks within the downtown area completed, especially from Brighton Park to the “Square.”



*Walking Audit participants walking along NC 281 toward Nelson Road*

#### **1.4.4 Social Media**

With the increased use of social media to connect with friends, follow user groups and communicate quickly, it was decided to use Facebook to receive input from the community. A Mint Hill Pedestrian Master Plan user group was created on Facebook. The intent behind the use of Facebook was to allow individuals the opportunity to post comments for everyone see them immediately, and be able to respond to. In addition, members of the Facebook group were encouraged to post pictures of areas throughout town that needed improvement. Town Staff and Consultants often published helpful websites and informative videos meant to educate the public and start discussion.

*“A pedestrian connection and crossing guard (for the kids not the cars) at Queens Grant Charter School is important. Any kids that walk today walk in the dirt along the driveway and the traffic is so bad.”*

*- Concerned Citizen -*

## 1.5 Vision and Goals

The input received from the public workshops, the stakeholder interviews, meetings with the Steering Committee, discussions with the Town staff and results from the questionnaire led to the development of a vision statement and key goals. The vision statement and the goals from this plan, along with the vision and goals identified in previous plans, have guided the development of the Pedestrian Master Plan and will continue to do so once the plan is adopted.

Mint Hill will promote a walkable and healthy community through the development of a comprehensive pedestrian network that is safe and accessible by all users, is environmentally sensitive, well planned and designed and connects citizens to major destinations within the town.

Vision Statement for the Pedestrian Master Plan

### 1.5.1 Pedestrian Master Plan Goals

The following are the goals that were developed during the process in order to achieve the vision statement that is listed above. These goals, like the vision statement, will continue to guide the implementation of the Pedestrian Master Plan.

- Create pedestrian crossings at key intersections.
- Improve connectivity throughout Mint Hill.
- Connect downtown to the surrounding neighborhoods through a network of safe sidewalks.
- Develop greenways and other pedestrian facilities that are environmentally sensitive.
- Improve connectivity to existing schools through the development of safe pedestrian infrastructure.
- Increase safety of neighborhood streets.
- Create pedestrian facilities that are convenient and easily accessible.
- Foster relationships with the development community to ensure the inclusion of pedestrian facilities within site plans.
- Increase pedestrian connectivity between existing and future subdivisions.
- Ensure that pedestrian facilities are attractive through proper maintenance.
- Develop a pedestrian system that is accessible by all users.

- 
- <sup>1</sup> **North Carolina Office of State Budget and Management:**  
[http://www.osbm.state.nc.us/ncosbm/facts\\_and\\_figures/socioeconomic\\_data/population\\_estimates/demog/muntot09.htm](http://www.osbm.state.nc.us/ncosbm/facts_and_figures/socioeconomic_data/population_estimates/demog/muntot09.htm).
- <sup>2</sup> **U.S. Census Bureau – American Community Survey:**  
<http://www.census.gov/compendia/databooks/2010/tables/sma A-63.xls>
- <sup>3</sup> **The Journal of the American Medical Association:**  
<http://jama.ama-assn.org/cgi/content/full/303/3/235?ijkey=ijKHq6YbJn3Oo&keytype=ref&siteid=amainls>
- <sup>4</sup> **Pedestrian and Bicycle Information Center:**  
<http://www.walkinginfo.org/why/benefits.cfm>
- <sup>5</sup> **National Trails Training Partnership:**  
<http://www.americantrails.org/resources/benefits/BenefitsGrnwy.html>
- <sup>6</sup> **Pedestrian and Bicycle Information Center:**  
[http://www.walkinginfo.org/why/benefits\\_environment.cfm](http://www.walkinginfo.org/why/benefits_environment.cfm)
- <sup>7</sup> **Pedestrian and Bicycle Information Center:**  
[http://www.walkinginfo.org/why/benefits\\_quality-of-life.cfm](http://www.walkinginfo.org/why/benefits_quality-of-life.cfm)
- <sup>8</sup> **National Trails Training Partnership:**  
<http://www.americantrails.org/resources/benefits/homebuyers02.html>
- <sup>9</sup> **Pedestrian and Bicycle Information Center:**  
[http://www.walkinginfo.org/why/benefits\\_quality-of-life.cfm](http://www.walkinginfo.org/why/benefits_quality-of-life.cfm)





# **CHAPTER 2**

## **Existing System Conditions**



## MINT HILL PEDESTRIAN MASTER PLAN



# Existing Conditions

## 2.1 Overview

In order to develop a comprehensive pedestrian network, it is critical to have a thorough understanding of the existing conditions within the Town of Mint Hill. This includes all the existing sidewalks, greenways/trails, lighting, intersection treatments, programs and policies. This chapter outlines the existing conditions as well as the past and on-going planning efforts of the Town.

## 2.2 Existing Pedestrian Network

### 2.2.1 Overview

Like many communities across North Carolina, Mint Hill has developed from a small rural community into a vibrant suburban community with a small town character. Many of the roadways within Mint Hill were once farm-to-market roads; the result is that many of the roadways and intersections within the town do not contain the basic infrastructure needed for safe pedestrian travel. In fact, the only intersection within the town that has sufficient pedestrian amenities is the “Square” (NC 51/Lawyers Road intersection). This chapter will provide a detailed overview of the existing pedestrian system that exist on the major thoroughfares within the town. Figure 2.1 (Existing Pedestrian Network) illustrates the existing pedestrian facilities located throughout the town.

### 2.2.2 Existing Sidewalks

Sidewalks are a critical component of any successful pedestrian system and currently there are only a few areas within the town that contain significant segments. The largest number of sidewalks exists within the downtown overlay district, especially along NC 51. There are some sidewalks on the major thoroughfares, such as Idlewild, Margaret Wallace, Wilson Grove and Fairview Roads (see Figure 2.1). These sidewalks are spotty and do not create long continuous stretches of sidewalks, therefore creating an unsafe walking environment throughout most of the town. Based on input received from the Steering Committee and the public throughout this process, filling in the gaps between existing sidewalks, especially in the downtown area should be a priority for the Town moving forward.



*Newer subdivisions like Brighton Park are required to construct 5-foot sidewalks*

Many of the older subdivisions lack sidewalks due to the fact that sidewalks were not required during their construction. However, many of the newer subdivisions

have sidewalks and as the town continues to grow, more and more sidewalks will be built and connected to each other. As of today there is approximately

59,200 ft. or 11 miles of existing sidewalk along the major thoroughfares throughout the Town of Mint Hill.

### ***2.2.3 Existing Intersections and Crossings***

As mentioned earlier, sidewalks are a critical component of a successful pedestrian system. Equally important are pedestrian amenities at intersections, such as pedestrian signals, pedestrian lighting, painted crosswalks, access ramps and signage. Currently, the only intersection that contains these elements is the intersection of NC 51 and Lawyers Road (“The Square”). In addition to the basic elements listed above, the “Square” contains brick pavers instead of the standard crosswalk striping, enhanced landscaping and lighting to create a more pleasant and safe walking environment. This enhanced treatment is more expensive than traditional and more standard treatments and should only be used at a handful of intersections throughout the town. Chapter 3 explains in more detail the recommended treatments for the major intersections throughout the town.

In addition to signalized intersections, there are some non-signalized intersections that were identified as needing some treatments to improve pedestrian safety. These intersections are listed in Chapter 3, but are locations where there is an observed high volume of pedestrian crossings and something needs to be done to improve safety and visibility.



*Intersection treatments at the “Square” like this improve pedestrian safety*

### ***2.2.4 Existing Greenways***

Currently, the Town of Mint Hill does not own or operate any greenways within the Town limits. Some of the existing residential subdivisions have incorporated trails that provide interconnectivity and are used by the residents that live in them. These facilities are private and intended to be used only by the residents that live in the subdivision. However, as the Town begins to develop public greenways in the future, it will be important to consider connecting to these private trails. There are several opportunities throughout the town to utilize creeks, streams and sewer easements to develop greenways. Figure 3.1 in Chapter 3 illustrates where these corridors are located within the town.

### ***2.2.5 Destinations and Trip Generators***

There are various destinations within the town that people walk to everyday. In fact, input from the Steering Committee, the public and Town staff indicates

that more people would walk to their destinations if more sidewalks and other pedestrian amenities were provided. Some of the most common destinations that individuals are currently walking to are listed below. A comprehensive list of destinations can be viewed in the Questionnaire Results in Appendix A.

- Mint Hill Pavilion and Brighton Park
- Mint Hill Library
- Town Hall
- Mint Hill Village (Hawthorne’s New York Pizza, Subway, etc)
- Dunkin Donuts/Baskin Robbins
- The Hill Bar and Grill
- Wilgrove and Fairview Parks

## **2.3 Past and On-Going Planning Efforts**

### **2.3.1 Overview**

The Town of Mint Hill has taken and continues to take proactive steps towards planning for future development and growth. In addition to applying for and receiving a grant to conduct this pedestrian plan, the Town has completed and adopted a Downtown Master Plan, a Comprehensive Transportation Plan, a Unified Development Ordinance (UDO), a Downtown Overlay Code and is currently in the process of developing a Comprehensive Land Use Plan and a Small Area Plan for the Lawyers Road/I-485 area. Each of these documents has recommendations, and in the case of the UDO, requirements for pedestrian infrastructure. The development of the Pedestrian Master Plan was coordinated with each of these plans. These documents have been reviewed and summarized as they relate to the development of a comprehensive pedestrian network.

### **2.3.2 Downtown Master Plan**

The Downtown Master Plan was adopted in 2002 and was an update to the original Downtown Master Plan which was adopted in 1992. The recommendations outlined in this plan will ultimately help create a pedestrian friendly environment throughout the downtown area. Some of the recommendations from the plan include the following:

- *“Require the construction of multi-use paths along the designated corridors through the development process.”*

- *“Where connections have not been created due to development, identify other governmental resources to purchase easements/rights-of-way and construct multi-use paths.”*

There were also specific recommendations for a development proposal that was pending at the time. This property was referred to as the “Teeter” property. Recommendations that affected the pedestrian network in that area included:

- *“Interconnected street network designed for both the automobile and the pedestrian.”*
- *“Pedestrian-scaled neighborhood shopping center.”*
- *“Network of open spaces from playgrounds to greenways.”*
- *“Pedestrian crosswalks should be constructed across NC 51 to provide residents access to the Library and Charter School across the street.”*



**Downtown Master Plan illustration**

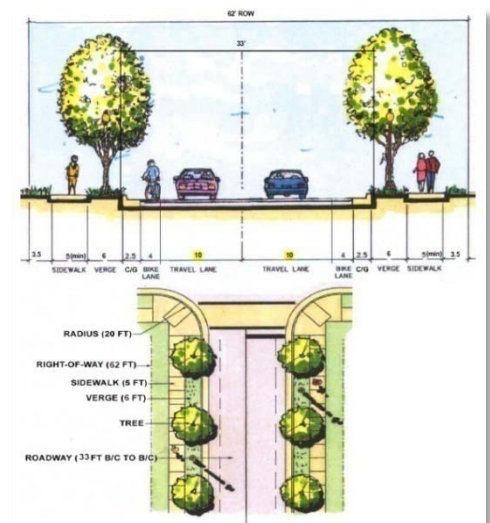
Lastly, recommendations were made regarding building design and the orientation and architecture of future buildings within the downtown area. More specifically, the plan says that *“Buildings should be close to the street to encourage social interaction and pedestrian scale.”*

### **2.3.3 Comprehensive Transportation Plan**

The Comprehensive Transportation Plan (CTP) provided an inventory of the existing pedestrian infrastructure which was extremely helpful in developing the Pedestrian Master Plan. The CTP also identified existing public transit corridors and strategic road corridors.

Currently there is one public transit route that is operated by the Charlotte Area Transit System (CATS). This is a single, fixed route that travels from Charlotte to Mint Hill. The route utilizes Albemarle and Lawyers Road and terminates at NC 51 and Lawyers Road and is called the Albemarle Road Express. There are several transit stops along Lawyers Road within the Town limits. It will be critical to consider these stops as sidewalks and other pedestrian amenities are constructed in the future along this corridor and other public transit corridors.

The CTP provided some general recommendations that sidewalk development should adhere to. Since that time, the Town’s UDO has been updated to incorporate these recommendations. Table 2.1 summarizes the recommended sidewalk guidelines as outlined in the CTP.



**Example Collector Cross Section from the CTP**

**Table 2.1: Sidewalk Recommendations from the CTP**

Category	Recommendation
Width	<ul style="list-style-type: none"><li>• Minimum of 5-feet in suburban locations.</li><li>• Minimum of 8-feet in Downtown Overlay District</li></ul>
Setback	<ul style="list-style-type: none"><li>• Within Curb &amp; Gutter – Minimum setback should be 6-feet</li><li>• No Curb &amp; Gutter – sidewalks should be located with the open drainage channel between the roadway and the sidewalk.</li></ul>
Material	<ul style="list-style-type: none"><li>• Generally, sidewalks should be concrete. Other decorative materials, however, should be permitted in areas as dictated by streetscape designs</li></ul>
Location	<ul style="list-style-type: none"><li>• Sidewalks should constructed on both sides of major/minor thoroughfares and collector streets. In instances where a greenway is shown for a corridor, the greenway takes the place of a sidewalk on one side of the street.</li></ul>

As mentioned earlier, the CTP identified eleven community strategic corridors that required closer consideration and study. The plan looked at each corridor in more detail and made recommendations for improving each. Table 2.2 on the next page summarizes the recommendations for each corridor as it related to the development of the Pedestrian Master Plan.

**Table 2.2: Strategic Corridor Recommendations**

Road Name	Buffer Width (ft)	Sidewalk Width
Bain School Road	6	5-8
Lawyers Road (South of Bain School Road)		10 (multi-use path – one side)
Lawyers Road (North of Bain School Road)	6	5-8
Bartlett Road	6	5
Blair Road/NC 51 (South of Truelight Church Road)	6	5-8
Blair Road/NC 51 (North of Truelight Church Road)	6	None
Brief Road	6	5
Idlewild Road	6	5
Lawyers Road (South of Nelson Road)*	6	8
Lawyers Road (North of Nelson)	6	5
Lebanon Road (North of Margaret Wallace Road)	6	5
Lebanon Road (South of Margaret Wallace Road)	6	5
Matthews-Mint Hill Road (North of Phyliss Lane)*	6	8
Matthews-Mint Hill Road (South of Phyliss Lane)	6	5
Thompson Road	6	5
Wilgrove-Mint Hill Road (South of Nelson Road)*	6	8
Wilgrove-Mint Hill Road (North of Nelson Road)	6	5
Wilson Grove Road	6	5
Fairview Road	6	5



Finally, the CTP outlined several key policy guidelines that should be used to create a unified network of pedestrian facilities in concert with new pedestrian development throughout the Town of Mint Hill. These guidelines are listed below.

- *Orient new commercial development to the pedestrian with accessible pedestrian walkways.*
- *Construct new residential development to be pedestrian friendly, with interconnected, grid-like street patterns and block lengths less than 660 feet in distance.*
- *Provide adequate pedestrian connectivity between new and existing developments.*
- *New residential, commercial and mixed-use developments should provide sidewalks on both sides of the street, plant trees that shade sidewalks and ensure an adequate buffer distance between traffic and off-street parking lots.*
- *Enact a policy to create pedestrian-friendly parking lots by encouraging shared parking between businesses and constructing sidewalks that provide safe routes from the parking lot to the commercial development.*
- *Create pedestrian linkages to existing and future transit operations.*

#### **2.3.4 Unified Development Ordinance**

There are several sections of the Town's Unified Development Ordinance (UDO) that affect pedestrian infrastructure and ultimately pedestrian mobility throughout the town. The following is a summary of those sections of the UDO that are directly related to the development of pedestrian infrastructure and will influence future pedestrian mobility throughout the town.

##### General Requirements

- The Town requires that new developments street network interconnects within a development and with adjoining development. This will help create a town-wide street network that will improve connectivity, providing pedestrians and cyclists the ability to walk and bike to their destinations.
- The Town also requires that a minimum of ten foot wide easements shall be provided for open or piped storm drainage, sanitary sewers, water lines and other utilities. The Town should consider utilizing easements corridors for the development of greenways. This will require close coordination with utility companies.

### Required Work on the Ground

- Sidewalks are required in all new development according to the following standards. In Non-Residential areas, sidewalks shall be constructed on both sides of existing arterial streets and secondary streets. They are required on one side of arterial and secondary streets where the street will not function, at the time the subdivision is approved, as an arterial or secondary street because of its lack of continuity.
- In Residential subdivisions sidewalks are required on both sides of the existing arterial and secondary streets. They are only required on one side of arterial and secondary streets where the street will not function, at the time the subdivision is approved, as an arterial or secondary street because of its lack of continuity. Sidewalks are also required to be constructed on one side of all classes of streets with the exception of the cul-de-sac “bulb provided that the sidewalk shall be located on the side of the street first subdivided.
- In addition, sidewalks shall be constructed on both sides of the streets that provide continuity through a subdivision or with such potential continuity through subdivision and adjacent areas that they may serve as general traffic access streets for the neighborhood. Sidewalks should also be constructed on streets that provide access to existing elementary schools, junior schools, high schools, colleges, and official sites for such schools; and streets that provide access to existing places of public assembly.

The outer edge of the sidewalk shall be located at the street right-of-way when the street functions as an arterial, secondary or general traffic access street. The inner edge of the sidewalk shall be located a minimum of five (5) feet from the curb.

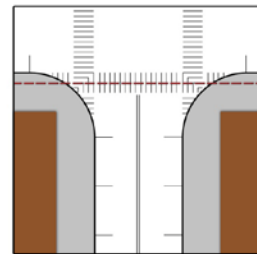
### **2.3.5 Downtown Mint Hill Overlay Code**

The following is a summary of sections of the Downtown Overlay Code that have a direct correlation to the Pedestrian Master Plan and pedestrian mobility within the Town of Mint Hill. The recommendations outlined below from the Downtown Overlay District are intended to improve the overall pedestrian network throughout the Downtown area.

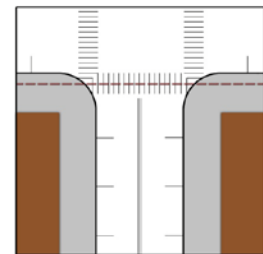
The Downtown Overlay Code encourages the development of a network of interconnecting streets that work to disperse traffic while connecting and integrating neighborhoods with the existing urban fabric of the Town. Equally as

important, the Code encourages the development of a network of sidewalks and bicycle lanes that provide an attractive and safe mode of travel for pedestrians and cyclists. The following are standards for streets and on-street parking areas within the Downtown Overlay District.

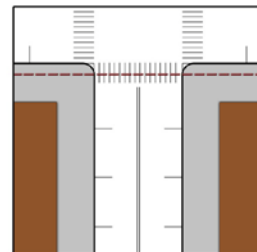
- Streets shall be designed as the main public space of the Town and shall be scaled to the pedestrian
- Street shall be bordered by sidewalks on both sides.
- Streets shall be designed with street trees planted in a manner appropriate to their function. Commercial streets shall have trees which compliment the face of the buildings and which shade the sidewalk. Residential streets shall provide for an appropriate canopy, which shades both the street and sidewalk, and serves as a visual buffer between the street and the home.
- All on-street parking provided shall be parallel. Curb or angle parking is permitted when the fronting buildings are more than 26 feet in height to ensure a safe and usable pedestrian realm and where traffic speeds are very low (20 mph or less).
- The use of traffic calming devices such as raised intersections, landscaping curb extensions, and traffic circles are encouraged as alternatives to conventional traffic control measures.
- Sidewalks shall be constructed along both sides of all streets except alleys and lanes. Cul-de-sacs and closes shall be reviewed on a site-by-site basis for this requirement. Residential sidewalks shall be a minimum of 5 ft in width. Sidewalks serving mixed use and commercial areas shall be a minimum of 8 ft in width (10-12 ft is preferable in front of shopfronts).
- Curb radii shall be designed to reduce pedestrian crossing



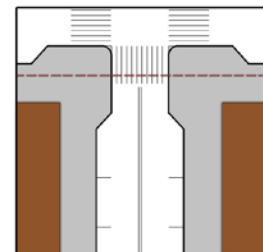
*Crossing distance 56-feet (17-meters) with 25-foot (7.6-meter) radius.*



*Crossing distance 44-feet (13.4-meters) with 15-foot (4.6-meter) radius.*



*Crossing distance 34-feet (10.4-meters) with 5-foot (1.5-meter) radius.*



*Crossing distance 22-feet (6.7-meters) with bumpouts*

times along all streets requiring sidewalks. In general, curb radii should not exceed 20 ft.

The following table provides an overview of the recommended cross-sections for each of the roadway typologies.

Road Type	Sidewalk Width (ft)	Buffer Distance	On-Street Parking (yes/no)	Speed (mph)
<b>Commercial Main Street</b>	16 (combined with buffer)	16 (combined with sidewalk)	Yes	25
<b>Street</b>	5	8	No	15-25
<b>Minor Street</b>	5	6	No	20

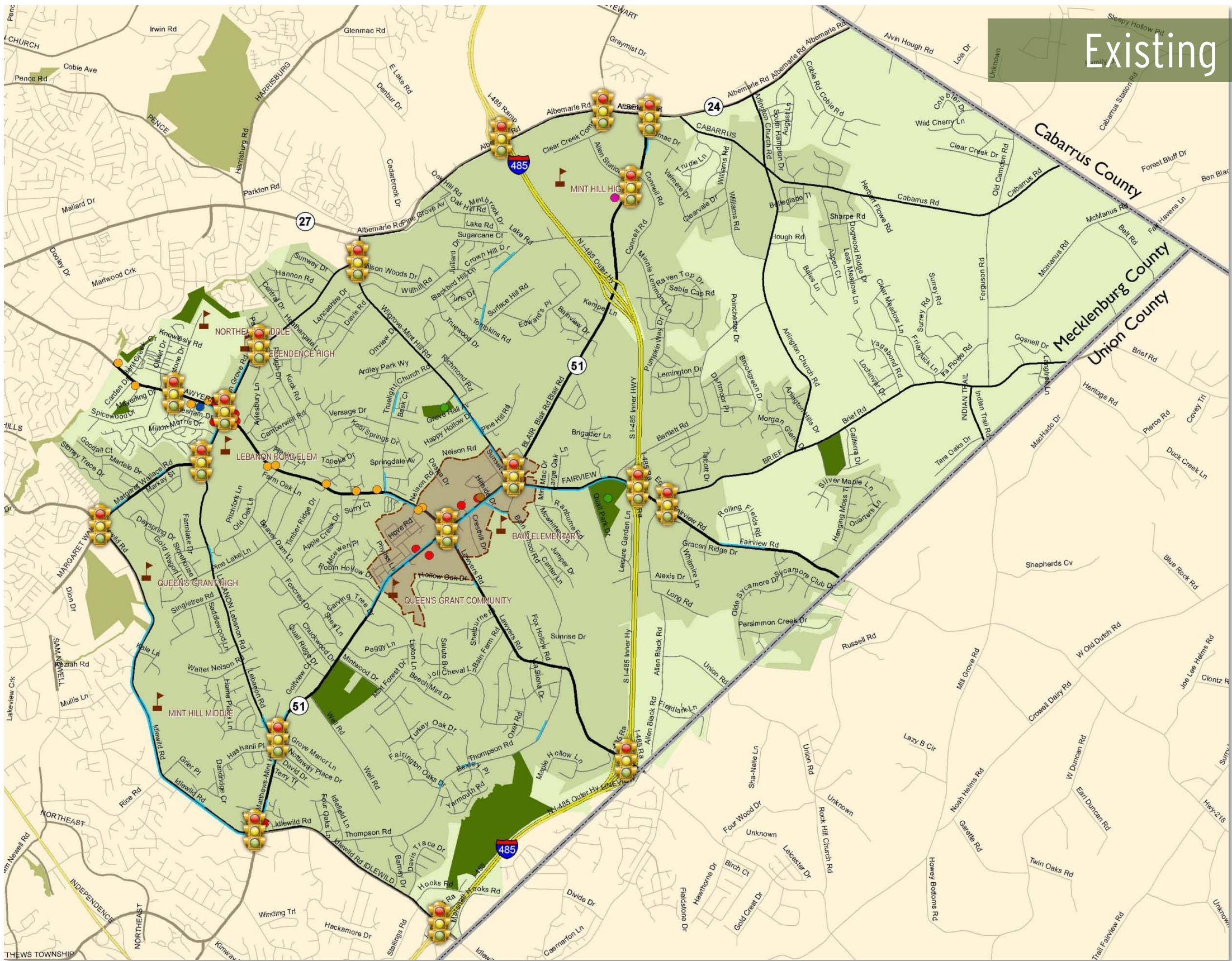
In addition to the street and sidewalks standards list above, the Downtown Overlay Code has specific standards for off-street parking areas. These standards have been created to improve the pedestrian environment and connectivity from the street to the building.

- Parking lots should be designed in such a way as to not dominate the frontage of pedestrian-oriented streets, interrupt pedestrian routes, or negatively impact surrounding neighborhoods.
- Parking areas shall not abut pedestrian-oriented street intersections or civic buildings, be adjacent to squares or parks, or occupy lots which terminate a vista.
- No off-street parking area shall be located within any front yard except for single-family residential uses. All off-street parking spaces for multi-family buildings shall be in the rear yard only.
- Off-street parking areas shall be designed so that parked vehicles do not encroach upon or extend onto public rights-of-way, sidewalks or strike against or damage any wall, vegetation, utility, or other structure.
- Handicap parking spaces where required by building standards shall be located as close to the primary entrance as possible.
- The joint use of shared off-street parking between two uses may be made by contract between two or more adjacent property owners. Adjacent lots shall be interconnected where practical. This will help reduce the amount of driveway cuts; reducing the amount of conflict points between motorists and pedestrians.



Figure 2.1:

# Existing Pedestrian System Map



- Town Square
- Mint Hill Schools
- Existing Sidewalks
- Downtown Overlay
- Existing Signalized Intersections

## Existing Destinations

- Commercial
- Institutional
- Medical
- Museum
- Park
- Transit Stops

## Downtown Overlay

## County Boundary

## Thoroughfares

- Existing Freeway
- Existing Major Thoroughfare
- Existing Minor Thoroughfare
- Local Street

- Parks
- Town Limits
- Planning Area





**MINT HILL PEDESTRIAN MASTER PLAN**



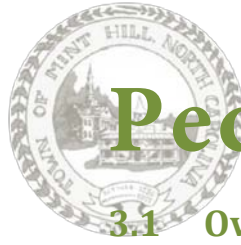
# **CHAPTER 3**

## **Pedestrian System**



## **MINT HILL PEDESTRIAN MASTER PLAN**





# Pedestrian System

## 3.1 Overview

The recommended pedestrian infrastructure outlined in this chapter will ultimately improve pedestrian mobility throughout the Town of Mint Hill. The Plan is based on an inventory of existing conditions, the communities input through the public workshops, the online questionnaire, and the Vision and Goals that were developed and refined throughout the process. This chapter provides an overview of the physical aspect of the Pedestrian Master Plan, which includes:

- Proposed sidewalks
- Proposed greenways
- Signalized and non-signalized intersection improvements

## 3.2 Methodology

The recommendations in this Plan were developed by thoroughly reviewing past planning efforts, inventorying the existing pedestrian facilities and gathering public input. A base map was created to better understand the physical conditions of the existing pedestrian system. The base map included various elements, including existing destinations, such as schools, churches, retail/commercial centers and parks. In addition, existing sidewalks were mapped by using a combination of information from the Comprehensive Transportation Plan and aerial photography that was provided by the Town and what was available online.

Site visits were then conducted to confirm the location and condition of the existing pedestrian facilities. The site visits were vital in determining current intersection conditions and to map the existing gaps in the sidewalk network.

## 3.3 The Pedestrian System

### 3.3.1 Overview

As mentioned previously, the Pedestrian System is a reflection of the needs and desires of the community and consists of sidewalks, greenways and intersection improvements. The Pedestrian System Map can be seen in Figure 3.1. This portion of the chapter will provide a summary of each component of the Pedestrian System, but to see a complete listing of the pedestrian improvement projects, please see the Pedestrian Project Matrix in Appendix B.

### **3.3.2 Proposed Sidewalks**

The proposed sidewalk improvement projects that have been illustrated in Figure 3.2 are a combination of information gathered from the Comprehensive Transportation Plan and input from the public. One of the major themes throughout the development of this plan was the need to complete the sidewalks along NC 51 within the Downtown Overlay District as well as improve connections and pedestrian access to schools. There are many smaller sidewalk projects that the Town can construct in the short-term that would greatly improve pedestrian mobility and allow citizens to walk to destinations instead of having to drive.

There are also many areas within the town that don't have any sidewalks, such as Lawyers Road north of Wilson Grove Road. There is a total of approximately 84 miles of proposed sidewalks throughout the Town of Mint Hill. A complete listing of all of the proposed sidewalks can be found in the Pedestrian Project Matrix in Appendix B.



### **3.3.3 Proposed Greenways**

Greenways are most commonly known as 10-foot wide asphalt trails that follow stream/creek corridors that provide continuous and uninterrupted walking and biking paths. In addition to providing a means of transportation, greenways serve other functions.

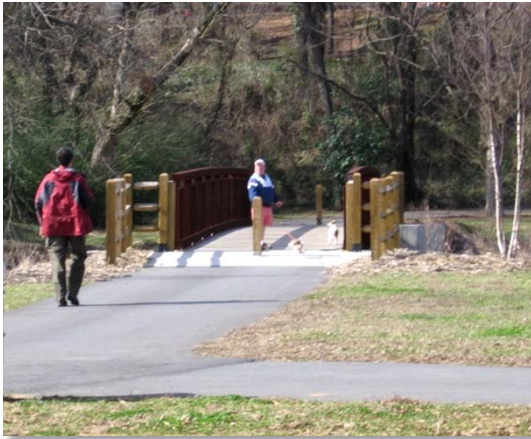
- Improve the economy through tourism and civic improvement
- Preserve and restore open space
- Provide opportunities for physical activity to improve fitness and mental health
- Improve water quality by creating a natural buffer for storm runoff

The proposed greenway system that is illustrated in Figure 3.3 is a combination of recommendations from past and ongoing planning initiatives as well as feedback from the Steering Committee and the public during this process.

As Figure 3.3 shows, there are four different categories of greenways:

- Mecklenburg County Greenways
- Comprehensive Transportation Plan Greenways
- Comprehensive Land Use Plan Greenways
- Proposed Connector

The greenways depicted in Figure 3.3 are conceptual lines that primarily follow the centerline of creeks and streams. These greenway corridors identify preferred corridors of travel by the community and are subject to change with future planning and design. These corridors have been identified because they make connections between origins and destinations within the town that are important to the citizens. Some of the greenways provide connectivity between schools and parks, while others create connectivity from residential subdivisions to parks and schools. It should be noted that the Comprehensive Land Use Plan has not been adopted by the Town of Mint Hill, therefore the greenways shown in Figure 3.3 from this plan are subject to change. There is a total of approximately 34 miles of greenways proposed in this Pedestrian Master Plan and the complete list is provided in Appendix B.



*Image of Little Sugar Creek in Charlotte, NC*

### **3.3.4 Intersection Improvements**

As mentioned earlier, only one intersection (NC 51 and Lawyers Road), known locally as the “Square,” currently has sufficient pedestrian amenities. This intersection was upgraded in 2006 to include pedestrian signals, enhanced landscaping, pavers for the sidewalks and the crosswalks and pedestrian lighting. These improvements enhanced the pedestrian safety as well as the overall aesthetics of the intersection.

There are a total of 17 signalized intersections (see Table 3.1) that currently exist within the Town of Mint Hill. Each intersection, except for the Square, will require some level of improvement in the future to better facilitate pedestrians through the intersection. These improvements should include, but are not limited to the following:

- Curb ramps
- Pedestrian signal controls
- Painted crosswalks
- Improved lighting
- Sidewalks

Chapter 5 (Facility Standards and Guidelines) provides a detailed description of these treatments and others that are necessary to improve pedestrian safety and



mobility. Of the 17 signalized intersections listed in Table 3.1, four are considered to be more visible and serve as gateways into the town. These intersections are referred to as Primary Signalized intersections in this plan and are highlighted with an asterik in the Table 3.1. These intersections should be enhanced with additional landscaping, signage and pedestrian lighting, similar to that at the Square.

**Table 3.1: Signalized and Non-Signalized Intersections**

Intersection Type	Intersection Location
Signalized	NC 51 / Idlewild Rd*
	NC 51 / Lebanon Rd
	NC 51 / Lawyers Rd*
	NC 51 / Wilgrove-Mint Hill Rd*
	NC 51 / Albemarle Rd
	Margaret Wallace Rd / Idlewild Rd
	Margaret Wallace Rd / Lebanon Rd
	Lebanon Rd / Lawyers Rd*
	Wilson Grove Rd / Wilgrove- Mint Hill Rd
	Albemarle Rd / Wilgrove-Mint Hill Rd
	Albemarle Rd / I-485
	Albemarle Rd / Rocky River Church Rd
	NC 51 /Allen Station Dr
	Wilson Grove Rd / Patriot Dr
	Lawyers Rd / I-485
	Fairview Rd (NC 218) / I-485
Non-Signalized	NC 51/Bain School Road
	NC 51/Mint Hill Library

***Primary Signalized Intersections***

Based on input from the Steering Committee, Town staff and the public, there has been an increase in pedestrian crossings at two specific non-signalized intersections along NC 51.

The first location is the intersection of Bain School Road and NC 51. According to input that was received during the planning process, individuals are crossing NC 51 at this location to access the Mint Hill Historical Society as well as other destinations. Currently, there are no plans to fully signalize this intersection, therefore something should be considered to improve the safety of the pedestrian crossing at this location.



**Intersection of Bain School Road and NC 51**



**NC 51 near the Mint Hill Library**

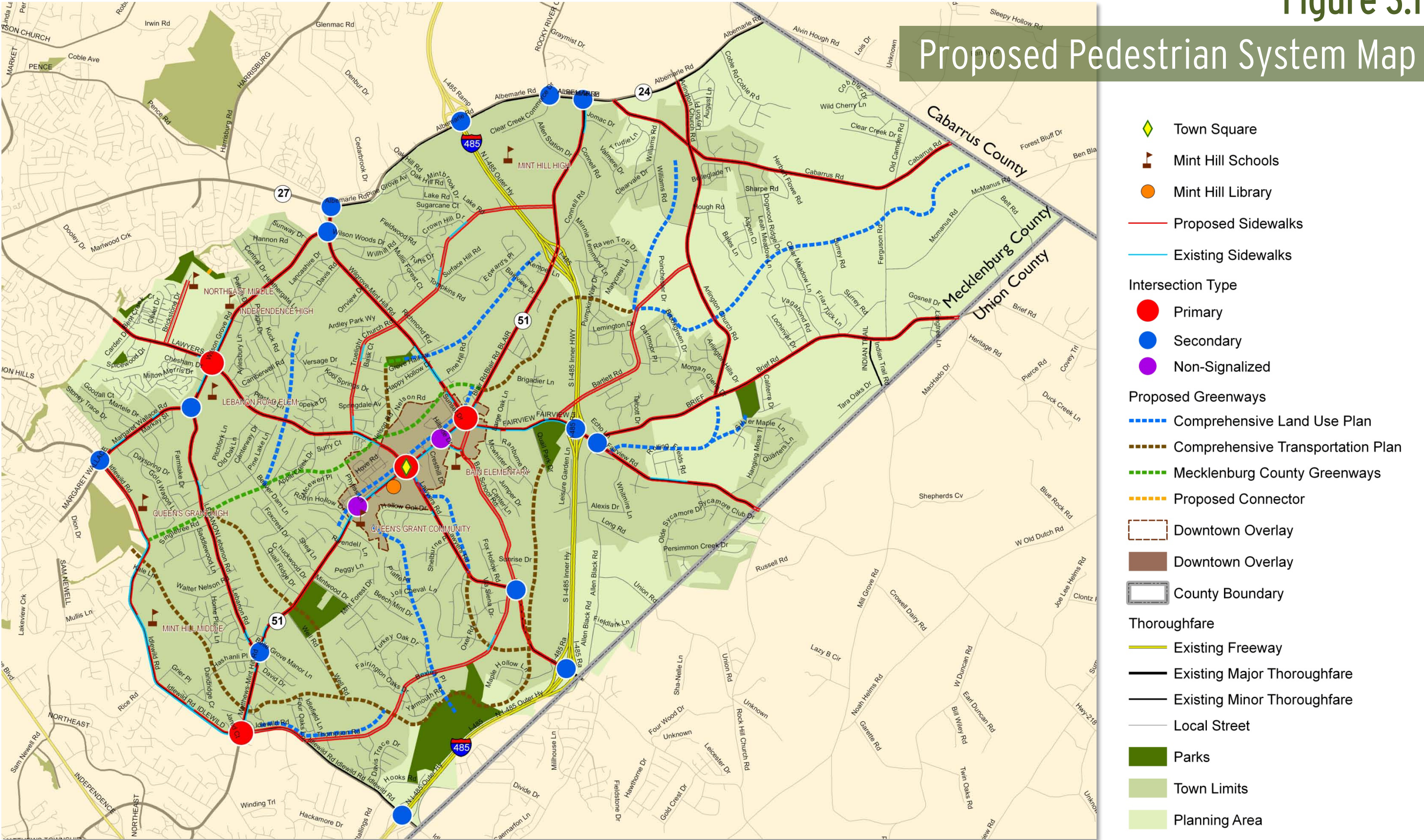
Similarly, individuals are crossing NC 51 to access the Mint Hill Library, which is located south of Lawyers Road. The Town of Mint Hill should work with NCDOT to create a solution that will address the safety concerns that currently exist at each of these non-signalized intersections.



## MINT HILL PEDESTRIAN MASTER PLAN



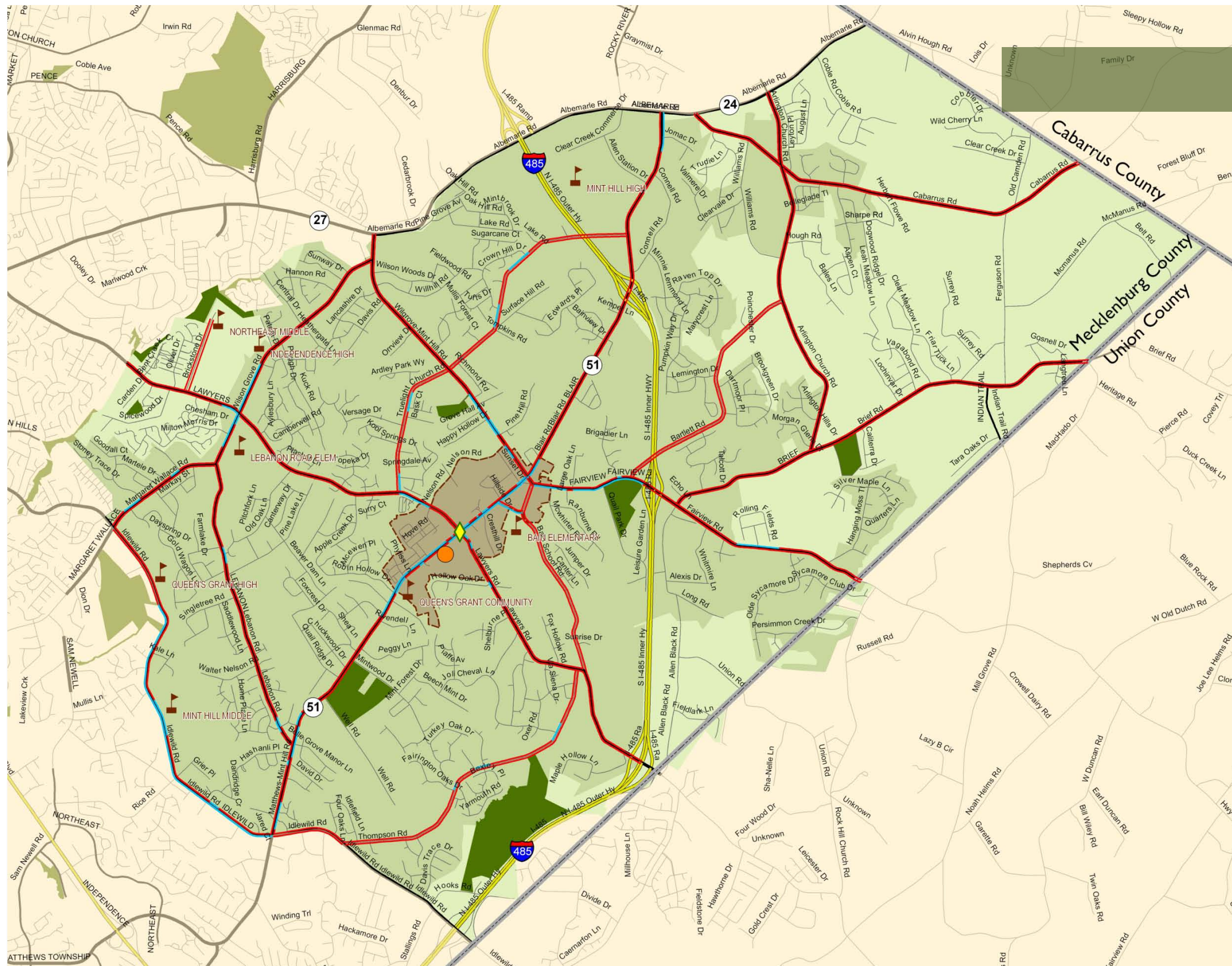
Figure 3.1





# Figure 3.2

## Proposed Sidewalks

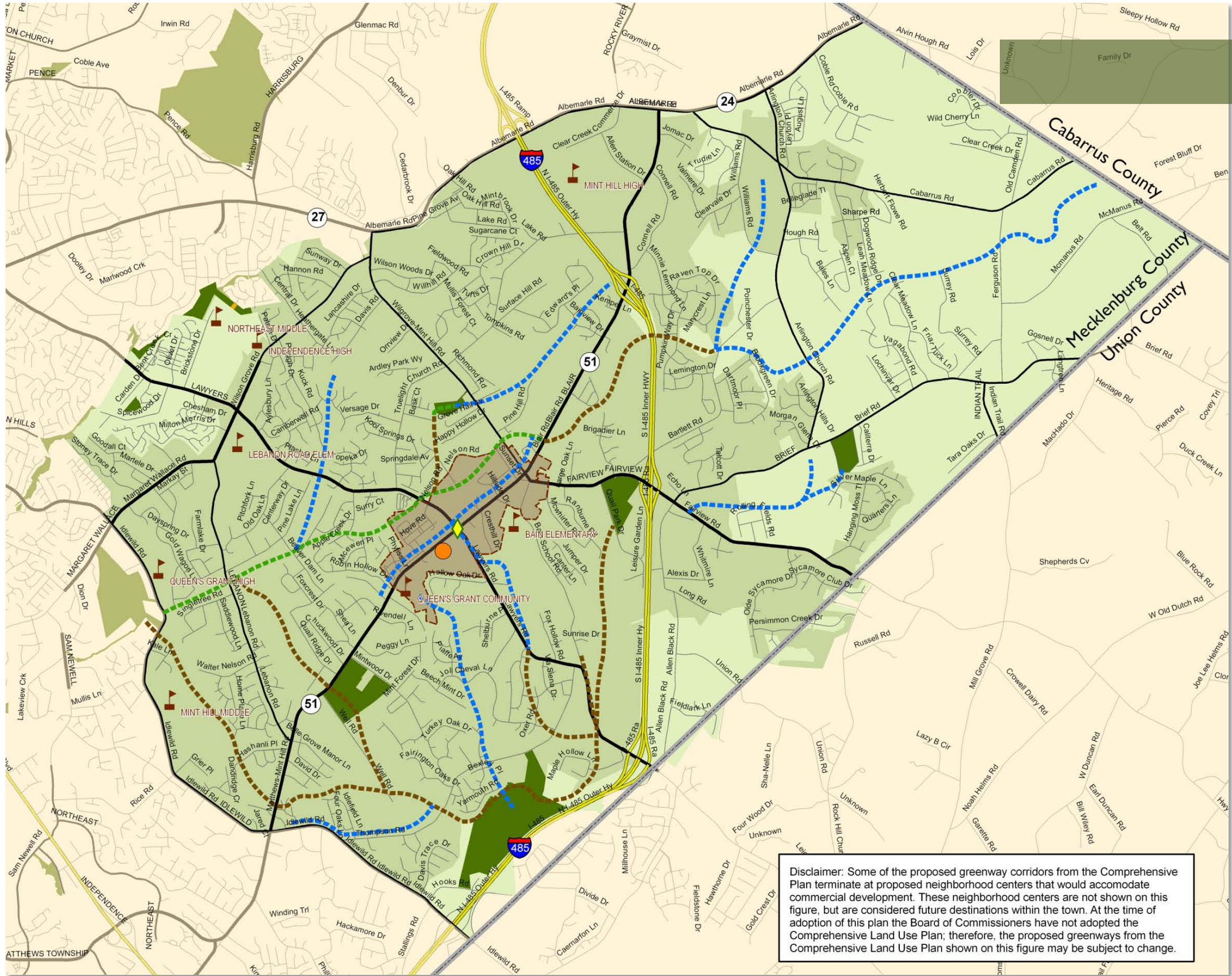


- ◆ Town Square
- ▲ Mint Hill Schools
- Mint Hill Library
- Proposed Sidewalks
- Existing Sidewalks
- Downtown Overlay
- Downtown Overlay
- County Boundary
- Thoroughfare**
  - Existing Freeway
  - Existing Major Thoroughfare
  - Existing Minor Thoroughfare
  - Local Street
- Parks
- Town Limits
- Planning Area



# Figure 3.3

## Proposed Greenways



- ◆ Town Square
- ▲ Mint Hill Schools
- Mint Hill Library
- Proposed Greenways
  - Comprehensive Land Use Plan
  - Comprehensive Transportation Plan
  - Mecklenburg County Greenways
  - Proposed Connector
- Downtown Overlay
- Downtown Overlay
- County Boundary
- Thoroughfare
  - Existing Freeway
  - Existing Major Thoroughfare
  - Existing Minor Thoroughfare
  - Local Street
- Parks
- Town Limits
- Planning Area





**MINT HILL PEDESTRIAN MASTER PLAN**

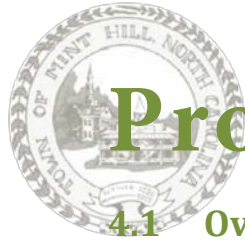


# **CHAPTER 4**

## **Programs and Policies**



## MINT HILL PEDESTRIAN MASTER PLAN



# Programs & Policies

## 4.1 Overview

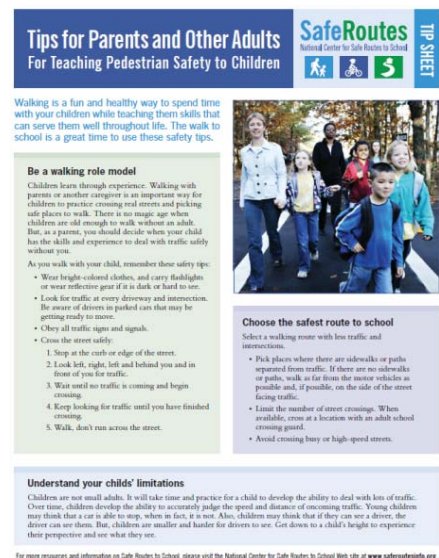
The successful implementation of the Mint Hill Pedestrian Plan cannot happen without a strong foundation of policies and programs. Chapter 3 provided an overview of the physical improvements that are required to create a well connected and more walkable community. The long-term success of the Plan must consider how these physical improvements will be implemented through various programs and policies. This chapter summarizes the existing programs and policies of the Town, and provides guidance on how to strengthen them.

## 4.2 Programs

### 4.2.1 Public Education

There are several types of media and public forums that can be used to inform and advocate for safe pedestrian travel. It is recommended that the Town encourage the development of the following to inform the public:

- **Local Advocacy Group:** The Town of Mint Hill should form an official Transportation Committee or similar committee that will oversee the implementation of the recommendations of this plan. The Steering Committee that oversaw the development of this plan could assume this role until an official committee is formed. This Committee will supplement the staff by promoting safe pedestrian travel and coordinating education and outreach opportunities.
- **Educational Materials:** The Town should develop an educational pamphlet that provides basic information on safe pedestrian behaviors, rules and responsibilities. This pamphlet could be distributed to residents through home owners associations (HOA), the Mint Hill Chamber of Commerce, and at annual town events. The pamphlet could also be available on the Town's web page and printed in local newsletters, newspapers or routine mailings, such as utility bills, as a way to reach out to the public. The North Carolina Department of Transportation has grants that Town can apply for to assist in the development and production of educational



materials. This is further explained in Chapter 6: Implementation Strategies. In addition to NCDOT, there are several national resources, such as the National Center for Safe Routes to School that the Town should utilize in developing educational materials.

- **Web-Based Education:** Mint Hill should continue to utilize its web page and the Facebook user group to the fullest extent possible to allow citizens to download useful information regarding the Plan, such as pedestrian laws, safety tips, or maintenance request forms.
- **Local Events:** The Town hosts several events throughout the year, including, Mint Hill Madness, Discover Mint Hill, Movies on the Lawn, June Tunes, Veterans Day Breakfast and the Christmas Tree Lighting. These events are ideal venues that allow Town staff and/or the local advocacy groups to hand out educational materials to the public. These events are also ideal for staff and the local advocacy group to interact with citizens and answer any questions, as well as solicit input from the general public on the implementation of the Pedestrian Plan.

Annual town events like Mint Hill Madness provide the Town leaders with an opportunity to educate the community and increase awareness about pedestrian issues within the town. These functions can also enable the Town to plan pedestrian-friendly activities that promote both physical activity and social interaction. The Town should approach Fit City Challenge, the Mecklenburg County Health Department and other local health and wellness groups, to partner in these events.

In addition to the Town's own local events, there are several different pedestrian oriented programs and initiatives that have been developed by affinity groups throughout the nation that can be easily implemented throughout Mint Hill.

- **Walk a Child to School Initiative:** The Walk a Child to School Initiative, supported by advocates nation-wide, emphasizes the importance of providing children with an opportunity to walk or bike to school in a safe environment. In 2010 there were a total of 84 schools registered in North Carolina for the event. <http://www.walktoschool-usa.org>
- **International Car Free Day:** International Car Free Day is an event organized by communities throughout the world with a common goal of taking cars off the streets for most of the day. The event takes place



*Annual Town events are ideal for promoting the Pedestrian Plan*



every September 22 and cities like Carborro have participated in the event to promote alternative forms of transportation.

<http://www.worldcarfree.net/wcfd/>



- National Trails Day: The American Hiking Society developed National Trails Day to inspire communities with to use their trails, celebrate their community, appreciate wildlife, and thank all of the people who built and maintain trails within the town.

<http://www.americanhiking.org/NTD.aspx>



- Citizen Watch Groups: Citizens using on- and off-road facilities are more aware of facility maintenance problems or suspicious activities at certain areas. The community should be encouraged to report any concerns to Town Staff so that issues can be resolved. This can be done by providing a feedback page on the Town's web-site or by forming a citizen's watch group with a liaison on the Pedestrian Plan Committee or Transportation Committee as discussed earlier in this section.



- Public Art Program: Public art along pedestrian corridors can involve local artisans and create a sense of community pride. Organizations like "Project for Public Spaces" (<http://www.pps.org/>) are dedicated to helping communities create a "public place," by providing mobile training workshops and free resources. Also, the Mint Hill Historical Society and the Mint Hill Arts can partner with the Town to educate the community about preserving the history of Mint Hill.
- Walking/Running Clubs: To promote ongoing wellness, area businesses and schools can create running and walking clubs. These programs can be used to increase pedestrian activity and social interaction among classmates and co-workers. Participants can meet before, during or after work on designated days of the week. Groups with a common thread, like new mothers or senior citizens, can also create clubs, resulting in a strong personal and community bond.
- Walk-to-School: Children and their guardians may choose to participate in programs like the Walking School Bus where neighbors walk to school together, similar to a school bus or carpool. One or more adults volunteer to rotate walking to designated points along the route to school. This is a simple and fun way to encourage students to stay active, while adults can worry less about their child's safety. Schools that are close in proximity to neighborhoods, like Mint Hill Middle, Bain Elementary and Lebanon Elementary could greatly benefit from a program like this.

#### **4.2.2 Staff Education**

The Town's staff must be properly educated on the most up-to-date pedestrian laws and design requirements from NCDOT and AASHTO. Annual internal training sessions will educate Town staff on the latest innovations in pedestrian standards. This training should include the planning, design, development review, construction, and maintenance aspects of the transportation and development process. The planning and public works staff should also incorporate pedestrian issues into their daily tasks. Web based training, such as webinars, are inexpensive and very accessible that the Town should utilize. It is also recommended that the Town join local and/or national organizations, such as the Association for Pedestrian and Bicycle Professionals. These organizations are very useful in answering questions and providing assistance with pedestrian and bicycle related issues. Also, as pedestrian improvement projects are implemented, the Town's Police Department should be informed about the new pedestrian facilities. Local law enforcement officials will be used to ensure new and existing facilities are functioning efficiently and safely.

#### **4.2.3 Charlotte-Mecklenburg Public School System**

The Town of Mint Hill will need to work to strengthen their partnership with the Mecklenburg County Board of Commissioners and the Charlotte-Mecklenburg Public School System to improve pedestrian mobility near schools. Without the Mecklenburg County Board of Commissioners and the Charlotte-Mecklenburg Public School System's involvement, pedestrian improvements in and around schools cannot be realized.

One way to improve the pedestrian infrastructure and to enable community leaders, schools and parents to improve safety and encourage more children, including those with disabilities, to safely walk and bike to school is through NCDOT's Safe Routes to School grant program. Improvements could include high visibility crosswalks, lighting and more sidewalks that lead into the school properties. Many of the schools within Mint Hill do not have the necessary facilities for children to be able to walk safely to school. More information for Safe Routes to School can be found in Appendix E.

### **4.3 Policy Recommendations**

#### **4.3.1 Overview**

Chapter 3 provided an overview of the physical aspects of the pedestrian plan; however, this portion of Chapter 4 outlines policy recommendations that are aimed at strengthening the Town's current ordinance to ensure that the pedestrian projects are implemented. The Town staff should become familiar



with these policies to ensure that new development is incorporating pedestrian facilities in their projects.

Over the past several years the Town has worked diligently to incorporate pedestrian related policies into their development ordinance.

Currently the Town of Mint Hill is developing a Unified Development Ordinance, which will combine their existing zoning and subdivision regulations and other regulatory documents into one unified ordinance. The UDO is currently in draft form and has not been adopted; however there are several sections that have a direct correlation with the implementation of pedestrian facilities.

#### ***4.3.2 Unified Development Ordinance (UDO)***

The Town has been working on reorganizing and updating the Unified Development Ordinance (UDO) since 2007. The revised UDO is currently under review, but does contain several important pedestrian related requirements that will ensure a safer pedestrian environment and connected community. The Town should be recognized for their commitment to take proactive steps to ensure future development and redevelopment considers pedestrian facilities. The following are recommendations for strengthening the requirements that were summarized in Chapter 2.

##### **General Requirements**

- Connectivity between existing and new neighborhoods should not be limited to automobiles. Pedestrian/bicycle connectivity should be achieved whenever possible. The Town should also take steps to educate the community where future pedestrian/bicycle connections will be established. Placing a sign that reads, “Future Pedestrian/Bicycle Connection.”
- The Town should improve the language of the UDO General Requirements to allow the use of easements for the development of greenways (i.e. sewer easements).
- The Steering Committee discussed alternative funding sources for the development of pedestrian infrastructure and it was the consensus of the group to have the Town pursue a “fee-in-lieu” option within the Town’s UDO. This fee could be deposited into a separate fund that could be used for parks, greenways, trails or other open spaces or recreational improvements in the vicinity of the development that is paying the fees.

##### **Section 13: Standards of Design – Streets**

- When two cul-de-sacs are abutting each other, there should be some requirement for making sure at a minimum that there is a pedestrian/bicycle connection.

#### Required Work on the Ground

- Think about overall connectivity to community. If only required on one side, then how does that connect to adjacent neighborhoods and street networks? Think about intersections and how they are treated. Make sure the appropriate pedestrian facilities are provided at the intersections (painted crosswalks, pedestrian countdown signals with audible sound, signage, etc.)

### **4.3.3 Downtown Mint Hill Overlay Code**

In addition to the Town's UDO, the Town's Downtown Overlay Code has several requirements that have been developed that will improve pedestrian mobility throughout the downtown area. The following are recommended improvements to the requirements outlined in Chapter 2.

#### Street Types and Standards

- Overall, Section 5 has many good requirements for the Downtown area. The use of curb extensions not only acts as a traffic calming device, it also reduces the crossing distance for pedestrians. There should be some reference to the use of landscaped medians as refuge islands, especially for areas where there is the potential for mid-block crossings.

#### Design Standards

- The design standards require that sidewalks be at least 5-feet for residential areas and 8-feet for commercial area. This plan recommends that NC 51 be used for a greenway within the downtown area. Signage should be used along the sidewalks within the downtown area to educate users that this stretch of NC 51 is an overland connector.

The overland connector is meant to facilitate pedestrians and bicyclists (multi-use) along the corridor to connect to other greenways recommended throughout the town. It will be critical to educate the community about the intent of this connection. For bicyclists and pedestrians to co-exist on the same path, certain safety rules will need to be established and followed in order to maintain a safe environment for all users.




#### Parking Standards

- Shared parking can reduce the number of driveway cuts along a street, thus reducing the number of conflict points between a motorist and pedestrian. It is recommended that the Town pursue the reduction of driveways along NC 51 and other major thoroughfares. This will require coordination with business owners and private property owners, but will greatly improve pedestrian mobility and create a safer walking environment for the community.



## **MINT HILL PEDESTRIAN MASTER PLAN**



# **CHAPTER 5**

## **Standards and Guidelines**



## MINT HILL PEDESTRIAN MASTER PLAN



# Standards & Guidelines

## 5.1 Overview

This chapter provides the Town of Mint Hill with a compilation of standards and guidelines that should be used in developing future pedestrian facilities. The standards and guidelines included in this chapter adhere to national and state standards that have been defined by the American Association of State Highway Transportation Officials (AASHTO), the Manual on Uniform Traffic Control Devices (MUTCD) and the North Carolina Department of Transportation.

This chapter should be a guide for the development of new pedestrian facilities as well as retrofitting existing pedestrian facilities. The standards and guidelines should only be used as a reference and a licensed engineer should be consulted when designing and constructing future pedestrian facilities. The following list is not meant to be a comprehensive list, but is merely an overview of some of the standards and guidelines that are available through the aforementioned agencies. Additional guidance on planning, design and operation of pedestrian facilities can be found

- Crosswalks
- Curb Extensions
- Greenways
- Curb Ramps
- Lighting
- Mid-Block Crossings
- Sidewalks
- Signage
- Signalization
- Overpasses
- Underpasses

## 5.2 Crosswalks

Crosswalks are just one of many components needed to facilitate pedestrians safely across roadways. Crosswalks serve two basic functions:

- Inform motorists of the location of a pedestrian crossing so that they have time to lawfully yield to a crossing pedestrian
- Assure the pedestrian that a legal crosswalk exists at a particular location.

They are often used in conjunction with other pedestrian components, such as pedestrian signals, stamped asphalt, etc. In most cases, marked crosswalks alone should not be installed within an uncontrolled environment when speeds are greater than 40 mph.<sup>1</sup>

### 5.2.1 Type of Crosswalks

According to AASHTO, there are three basic types of marked crosswalks: (1) Transverse, (2) Longitudinal, and (3) Diagonal. Transverse is the most commonly used and the least expensive of the three crosswalks. These are typically used in areas where there are low traffic volumes and vehicular speeds.

The Transverse treatment can be used in conjunction with other types of treatment, such as stamped asphalt or brick pavers to increase the visibility of the crosswalk.

Longitudinal and Diagonal are often referred to as the “Ladder” or “High Visibility” and “Zebra.” These types of treatments increase the visibility of the crosswalk and can increase the safety of the pedestrian. They can be very useful for mid-block crossings, areas where traffic volumes are high, vehicle speeds are greater and around schools.

In some locations, such as schools, raised crosswalks are needed in conjunction with striped crosswalks to slow down or “calm” vehicular speeds. Raised crosswalks are typically located at mid-block locations and are used on 2-lane roadways with posted speeds less than 35 mph. Where raised crosswalks are used, detectable truncated dome warnings are needed at the curb lines and visible pavement markings are required on the roadway approach slopes.<sup>2</sup>

## 5.3 Curb Extensions

Curb extensions significantly improve pedestrian crossings by reducing the pedestrian crossing distance, visually and physically narrowing the roadway, improving the ability of pedestrians and motorists to see each other, and reducing the time that pedestrians are in the street.<sup>3</sup>

On streets that allow on-street parking, curb extensions extend the sidewalk or curb line out into the parking lane,



*Standard “Transverse” treatment example*



*Longitudinal or Ladder crosswalk example*



*Diagonal or Zebra crosswalk treatment*



which reduces the overall street width. They improve pedestrian crossings by reducing the pedestrian crossing distance and increase the visibility of the pedestrian for the motorist. Curb extensions narrow the road, forcing the motorist to slow down as they enter the intersection; therefore acting as a traffic calming device.

In general, curb extensions should only be implemented where on-street parking is allowed. They should extend the width of the parking lane, approximately 6-feet from the curb, and never encroach into travel lanes, bicycle lane or shoulders.<sup>4</sup>



***Curb extensions reduce the crossing distance and increase the safety of the pedestrian***

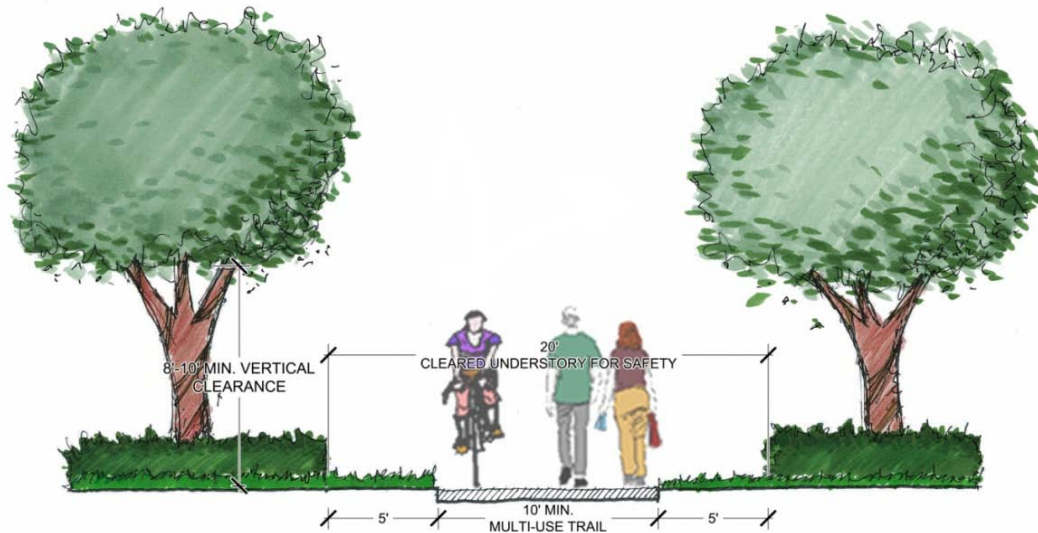
## 5.4 Greenways

Greenways are most commonly known as a vegetated natural buffer that can help improve water quality, reduce the impacts of flooding, and provide wildlife habitat. Greenways also provide recreation and fitness opportunities for individuals, serve as alternative transportation corridors, and can have positive economic impacts for communities. They are intended for all types of users, including walkers, joggers, bicyclists, roller bladders and other non-motorized modes of travel. Greenways are typically located adjacent to creeks and streams and should not be confused with sidewalks. Locating greenways adjacent to natural water features is not always feasible due to environmental or physical constraints, therefore more and more communities are working with utility companies to locate greenways within utility rights-of-way, such as sewer easements and overhead power lines.



***Greenways provide recreational opportunities and improve water quality.***

Greenways can be designed to accommodate a variety of users and can be paved or unpaved trails. They should be a minimum of 10-feet wide and include 2-foot graded shoulders. Greater widths of 12-14 feet are encouraged where significant traffic is anticipated.



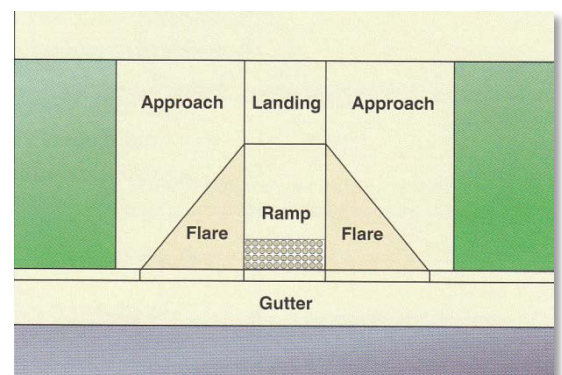
## 5.5 Curb Ramps

Curb ramps are an essential component of the pedestrian system and are used at intersections and mid-block crossings to facilitate pedestrians from sidewalks into roadways so that they can cross the street. Curb ramps are needed for individuals using wheelchairs and scooters, people pushing strollers and pulling luggage. The design of these ramps is critical to the safety of the pedestrian as well as the motorists. Items such as utility poles, traffic signs, signals, signal control boxes and street name signs should be located so that do not obstruct the pedestrian route.<sup>5</sup>

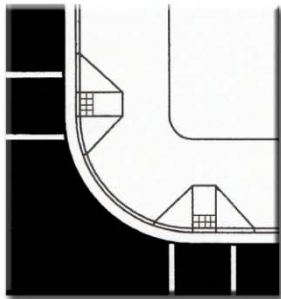
According to AASHTO there are four basic components of a standard curb ramp design:

- Ramps
- Landings
- Flares
- Gutters.

The construction of new curb ramps should be a minimum of 4-feet wide, not including the flared sides. Federal regulations require that the maximum grade of the curb ramp be no more than 8.33% or a ratio of 1:12. If the landing is less than the



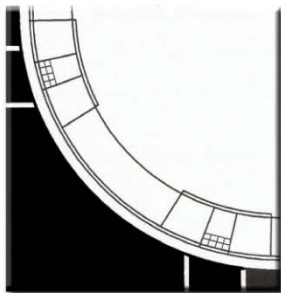
*Standard components of a curb ramp*



**Perpendicular Ramps**

recommended 4-feet deep, the slope of the flares may not exceed 8.33%. If the landing width is greater than 4-feet, then it is recommended that the slope of the flares be 10% so that tripping can be avoided.

Finally, all ramps are required to have detectable warnings located at the curb line for the full width of the ramp or walkway. The American with Disabilities Act Accessibility Guidelines (ADAAG) specifies that detectable warnings shall consist of raised truncated domes and specifies the dimensions and patterns of truncated domes to be used. For more information regarding the specific design criteria of curb ramps, please see the AASHTO Guide for the Planning, Design and Operation of Pedestrian Facilities and the ADAAG (<http://www.access-board.gov/adaag/html/adaag.htm>).

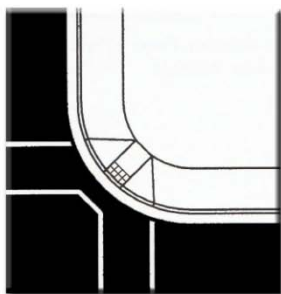


**Parallel Ramps**

The type of curb ramp to be used is based on the function of the sidewalk and border width, curb height, curb radius, and topography of the street corner. AASHTO identifies three basic types of curb ramps: perpendicular, parallel, and diagonal.

Perpendicular ramps are generally used where the curb radius is smaller and the vehicular speeds are relatively low. They are perpendicular to the face of the curb and available for each approach.

Parallel ramps require users continuing along the sidewalk to negotiate two ramp grades. It also requires careful attention to the construction of the landing at the bottom of the ramp in order to limit the accumulation of water and/or debris. A minimum of 4-feet is required between the two ramps (AASHTO: 86).



**Diagonal Ramps**

The Diagonal ramp is a single perpendicular ramp that is located at the apex of the corner. Diagonal curb ramps typically force pedestrians to enter the intersection before they are able to enter the crosswalk. This is especially dangerous for individuals with visual impairments because it directs them away from the crosswalk. In order to facilitate pedestrians into the appropriate crosswalk, a clear space should be provided that is a minimum of 4-feet from the edge of the ramp. This clear space should not extend into a travel lane.

## 5.6 Lighting

The proper lighting will greatly enhance the safety and experience for the pedestrian. It not only improves the overall safety, but is also improves vehicle and pedestrian operations. Insufficient lighting will deter pedestrians from using the facility, decreasing the value of the improvement. Lighting should be placed wherever there is significant pedestrian activity, particularly around schools, parks, residential areas and downtown. It is also very important to ensure that all pedestrian crossings are well lit and signed so that motorists are aware of the crossing.

The North Carolina Department of Transportation (NCDOT) recommends that on major arterials in urban or suburban areas, continuous street lighting should be provided. On wide arterials, they recommend the installation of double-sided lighting (both sides of the road). For new construction, street light poles should be located at least 6-feet from the curb face and out of the sidewalk. Whenever possible it is recommended that street lights, traffic signals and power distribution lines be located on a single pole. The Town of Mint Hill will need to coordinate with NCDOT on future roadway improvements to ensure that the proper lighting is provided for the pedestrian. Many of the roads within Mint Hill are owned and maintained by NCDOT; therefore, NCDOT will have a significant role in providing lighting along the major thoroughfares, such as NC 51.



*Lighting increases safety and visibility of the pedestrian*

## 5.7 Mid-Block Crossings

Mid-block crossings are useful where the distance between existing intersections is relatively far apart or where pedestrian related land uses are between intersections. They can also be useful in facilitating greenway users across the roadway. As mentioned in Chapter 3, there are two locations that have been identified as potential mid-block crossing candidates. The first location is at the intersection of Bain School Road and NC 51 and the other is along NC 51 near the library in downtown. When designed and constructed correctly, mid-block crossings allow pedestrians to cross one direction of traffic at a time and provide a refuge island halfway across the street. Unfortunately, each situation and available right-of-way is different. The two examples that have been identified have very little right-of-way to work with and widening the road to accommodate a refuge island may not be realistic. Other treatments can be evaluated to accommodate a mid-block crossing in these areas.



*Non-signalized mid-block crossing in Charlotte, NC*

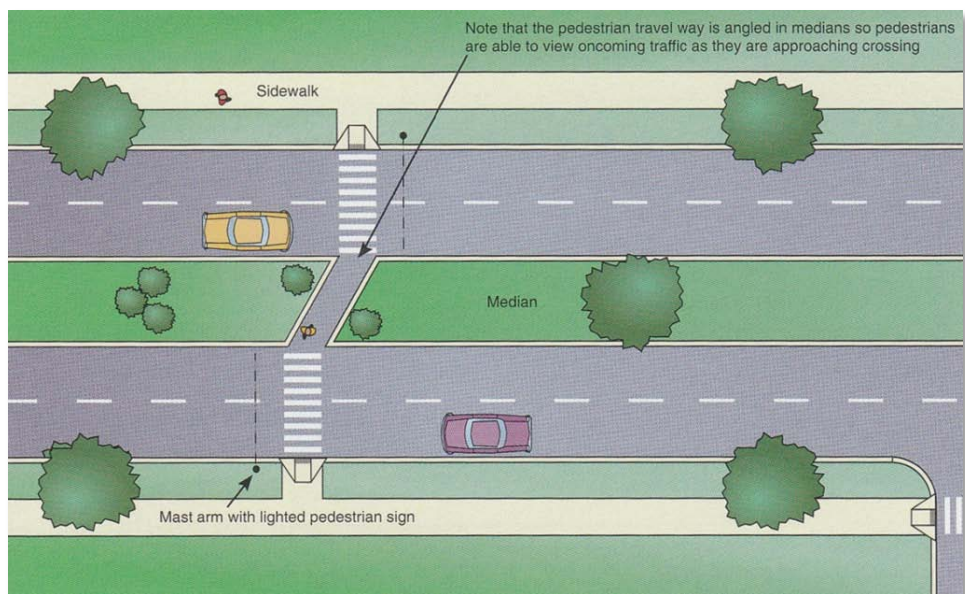
The placement and type of mid-block crossings is dictated by several factors including pedestrian volume, traffic volume, roadway width, traffic speed and type, desired paths for pedestrians, and adjacent land use.<sup>6</sup> Since mid-block crossings are not generally expected by motorists, they should be used only where truly needed and should be well signed and marked. In the case of the two mid-block crossings identified along NC 51, warning signs should be placed well in advance of the crossing and at the actual crossing.



The following are attributes where mid-block crossings can be most effective as defined by AASHTO:

- The location is already a source of a substantial number of mid-block crossings.
- Where a new development is anticipated to generate mid-block crossings.
- The land use is such that pedestrians are highly unlikely to cross the street at the next intersection
- The safety and capacity of adjacent intersections or large turning volumes create a situation where it is difficult to cross the street
- Spacing between adjacent intersections exceeds 660-feet
- The vehicular capacity of the roadway may not be substantially reduced by the mid-block crossing
- Adequate sight distance is available for both pedestrians and motorists.

In general, there are two types of mid-block crossings: signalized and non-signalized. Signalized mid-block crossing should be used where the crossing distance exceeds 60-feet.<sup>7</sup> The basic components necessary to complete a signalized mid-block crossing include curb ramps, striped crosswalks, “cut-through” in the median, pedestrian actuated signals next to the curb ramps and within the median. The “ladder” or “high visibility” crosswalk treatment is recommended for the crosswalks to improve visibility of the crossing.



The “cut-through” within the median should be angled if possible to force the pedestrian to make eye contact with oncoming vehicular traffic and to improve visibility for both the pedestrian and motorist. Landscaping can be incorporated into the median, but should be placed so as not to reduce visibility. To increase visibility and to warn motorists of a mid-block crossing, flashing pedestrian signals can be installed prior to the crossing. This additional measure could be very useful along NC 51 where traffic volumes and speeds are greater. Further study and analysis will need to be completed to determine the appropriate treatments for these crossings.

Non-signalized mid-block crossings can be located on roadways that have speeds less than 40 mph. These are generally used on 2-4 lane roadways that have low traffic volumes and/or low vehicular speeds. Again, it is recommended that the cut-through be angled to force eye contact and to increase the visibility of the pedestrian.

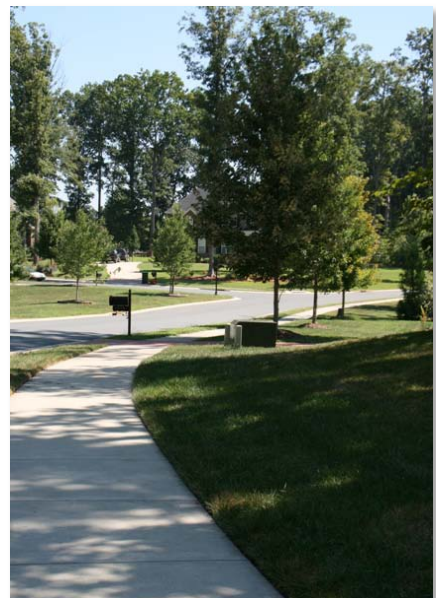


*Signalization like this might be necessary along NC 51 if a refuge island cannot be accommodated*

## 5.8 Sidewalks

Sidewalks are probably the most critical element of a successful pedestrian system. Sidewalks are typically located adjacent to the roadway and are ideally buffered with landscaping. They provide critical connections from residential areas to destinations, such as schools, libraries and recreational facilities.

According to AASHTO’s Guide for the Planning, Design and Operation of Pedestrian Facilities, sidewalks actually benefit both pedestrians and motorists by creating separation between the pedestrian and travel lanes. Sidewalks can vary in size, but should not be installed at a width less than 5-feet. The Town’s Unified Development Ordinance requires that in residential areas, sidewalks be a minimum of 5-feet and within the Downtown Overlay Zone they be a minimum of 8-feet. Sidewalks should be installed wherever pedestrian traffic is either anticipated or already occurring. New sidewalks must be accessible to and usable by persons with disabilities.



### 5.8.1 Sidewalks and Driveways

Conflicts between driveways and sidewalks are often unavoidable. Typically sidewalks located within residential areas experience fewer conflicts with driveways than sidewalks located in commercial areas. Commercial areas experience higher traffic volumes, therefore have the greatest potential for vehicle-pedestrian conflicts. The preferred treatment for driveway design, which is explained in detail in the Driveway Design section of the AASHTO Guide for the Planning, Design, and Operation of Pedestrian Facilities, places the driveway slope in the planting strip. By placing the driveway slope within the planting strip, it allows for a continuous level walkway.<sup>8</sup> There are a total of four basic and acceptable driveway designs that the Town should become familiar with for future development.



## 5.9 Signage

Signage plays a significant role in the safety of the pedestrian and motorist. For instance, signage should be provided for motorists in advance of a pedestrian crossing so that they have sufficient time to yield to the pedestrian.

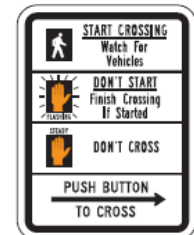
In general there are three basic types of signs that are used to direct pedestrian and vehicular traffic:

- Regulatory
- Warning
- Wayfinding.

### 5.9.1 Regulatory

Regulatory signs are used to inform motorists or pedestrians of legal requirements and should only be used when the legal requirement is not otherwise apparent. With the exception of STOP and YIELD signs, regulatory signs are rectangular in shape, usually contain a black legend on a white background, and are reflectorized or illuminated.<sup>9</sup>

Chapter 2 of the MUTCD provides specific guidance on the use of regulatory signs. Illustrated to the right are just some





examples of the regulatory signs used for pedestrian facilities. A complete list and description of each can be found at <http://mutcd.fhwa.dot.gov/pdfs/2009/part2b.pdf>.

### 5.9.2 Warning Signs

Warning signs are typically used to inform motorists and/or pedestrians of unusual or unexpected conditions, such as mid-block crossings. Warning signs should be placed far enough in advance to warn users and to provide sufficient response time. Warning signs, like Regulatory signs, are very distinctive and generally diamond-shaped with black letters or symbols on a yellow background. They too are reflectorized or illuminated to increase visibility.

As a rule, the placement of warning signs in advance of the subject condition should be based on the posted speed limit within the subject area. According to AASHTO, the pedestrian crossing sign located on the right (MUTCD W11-2) on the right serves two functions. First it provides advanced warning to motorists of possible pedestrian conflicts, and secondly, at a crosswalk it advises the motorists of the potential that a pedestrian may be attempting to cross. The examples to the right are just a sample of the many types of warning signs that are located in Chapter 2 of the MUTCD. Additional warning signs can be found in Chapter 2C, located at <http://mutcd.fhwa.dot.gov/pdfs/2009/part2c.pdf>.



### 5.9.3 Wayfinding

Wayfinding signs should be installed in locations where multiple destinations exist. Wayfinding signage should be easy to understand and should orient and communicate in a clear and concise manner. This type of sign is developed for both motorists and pedestrians and can be custom made based to match existing wayfinding that might exist.

The City of Charlotte recently developed a comprehensive wayfinding signage system that orients and directs motorists from the highway all the way down to the pedestrian at street level. An example of the pedestrian wayfinding signage from the City of Charlotte is provided to here.



## 5.10 Signalization

Traffic signals assign the right-of-way to vehicular and pedestrian traffic. Traffic signals benefit pedestrians by stopping vehicular traffic and allowing the pedestrian to cross the street safely. When traffic signals are installed and timed correctly, they can improve the efficiency of the overall transportation network. The MUTCD recommends that traffic signal timing for pedestrians be based on a

pedestrian crossing speed of 4-feet per second. However, this does not reflect the walking speeds of every user, especially children, persons with disabilities or elderly people. In order to accommodate all types of users, it is recommended that a pedestrian speed of 3-feet per second be used. See page 103 of AASHTO's Guide for the Planning, Design and Operation of Pedestrian Facilities for further guidance on traffic signal timing.

### 5.10.1 Pedestrian Signal Controls



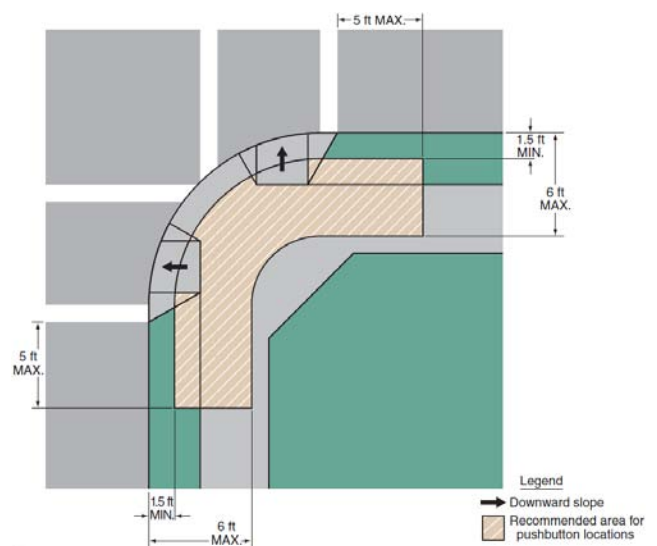
*Existing pushbutton signal at the Square*

Pedestrian signals controls or pushbuttons should be installed at all signalized intersections where pedestrian traffic is being facilitated across the roadway. Currently the only intersection within Mint Hill that has pedestrian signal controls is the "Square." The signal controls should be located within a reasonable distance from the curb ramp. The MUTCD provides the following guidance for locating pushbuttons at intersections where two control devices are located.

- Adjacent to a level all-weather surface to provide access from a wheelchair, and where there is an all-weather surface, wheelchair accessible route to a ramp
- Within 5-feet of the crosswalk extended
- Within 10-feet of the edge of the curb, shoulder, or pavement
- Parallel to the crosswalk to be used

The mounting height for pedestrian pushbutton detectors should be approximately 3.5-feet, but no higher than 4-feet as defined by the MUTCD. This allows easy access for those in a wheelchair to be able to reach and activate the signal. Individuals with visual impairments need audible or tactile cues to assist them when crossing a roadway. This type of signal is known as "accessible pedestrian signal" and should be used wherever a pedestrian actuated signal exists. The MUTCD provides very specific guidance on pedestrian signals. This information can be found at:

<http://mutcd.fhwa.dot.gov/pdfs/2009/part4.pdf>.

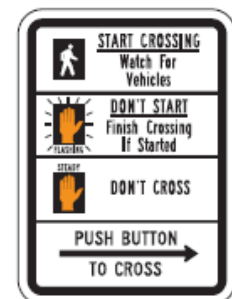
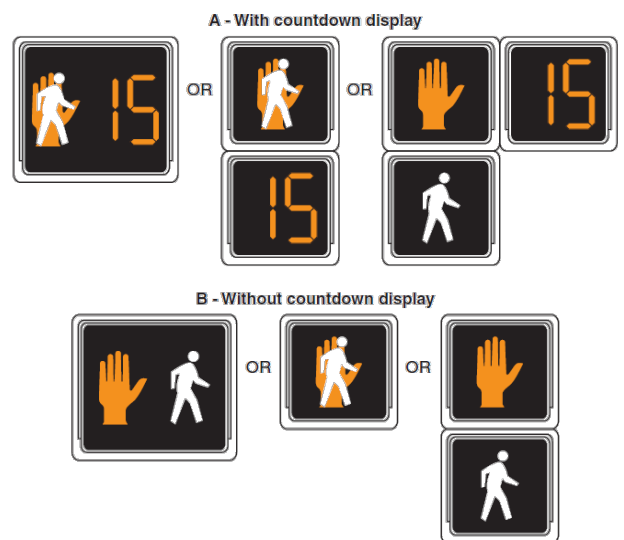


### 5.10.2 Pedestrian Signal Heads

Pedestrian signals heads provide special types of traffic signal indications exclusively intended for controlling pedestrian traffic. These indicators consist of illuminated symbols of a **WALKING PERSON** (symbolizing **WALK**) and an **UPRAISED HAND** (symbolizing **DON'T WALK**). The signal heads should be mounted no lower than 7-feet, but no higher than 10-feet above sidewalk level. The signal head should also be mounted so that it is clearly visible for pedestrian crossing from the opposite side of the roadway. The MUTCD states the following regarding pedestrian signal head indicators:

- A steady **WALKING PERSON** (symbolizing **WALK**) signal indication means that a pedestrian facing the signal indication is permitted to start to cross the roadway in the direction of the signal indication, possibly in conflict with turning vehicles. The pedestrian shall yield the right-of-way to vehicles lawfully within the intersection at the time that the **WALKING PERSON** (symbolizing **WALK**) signal indication is first shown.
- A flashing **UPRAISED HAND** (symbolizing **DONT WALK**) signal indication means that a pedestrian shall not start to cross the roadway in the direction of the signal indication, but that any pedestrian who has already started to cross on a steady **WALKING PERSON** (symbolizing **WALK**) signal indication shall proceed to the far side of the traveled way of the street or highway, unless otherwise directed by a traffic control device to proceed only to the median of a divided highway or only to some other island or pedestrian refuge area.
- A steady **UPRAISED HAND** (symbolizing **DONT WALK**) signal indication means that a pedestrian shall not enter the roadway in the direction of the signal indication.
- A flashing **WALKING PERSON** (symbolizing **WALK**) signal indication has no meaning and shall not be used

According to AASHTO and the MUTCD, research indicates that many pedestrians don't understand the meaning of these indicators; therefore educational signage can be placed near the pushbutton. The MUTCD recommends the use of R10-2 through R10-32P to educate pedestrians who may not comprehend the meaning of the pedestrian indicators. These can be found in Chapter 2B of the MUTCD.



## 5.11 Overpasses

There are situations where pedestrian crossings cannot be accommodated “at-grade” and other ways have to be explored to safely facilitate pedestrians across the roadway. These are most commonly known as “overpasses” or “underpasses” and can be very expensive to build. Typically, overpasses are created by utilizing pedestrian bridges and are used to cross major obstacles such as railroads, highways and even rivers and streams.



*Crossings such as this one over Little Sugar Creek are very common along greenways*

Many times providing ramps or steps can be challenging, especially if acquisition of private property is required. Overpasses need to either provide elevator access or meet Americans with Disabilities Act (ADA) ramp criteria for maximum slope (8.33%), level landings for every 30-inch rise in elevation, and handrails on both sides (AASHTO: 97). Overpasses or bridges must also maintain specific vertical and horizontal widths. The minimum inside width of a pedestrian bridge should be 8-feet, however if the bridge is enclosed

to prevent dropping of debris onto the roadway below, the visual tunnel effect may require widening the bridge to 14-feet to provide a feeling of security for all bridge users.<sup>10</sup>

## 5.12 Underpasses

Similar to overpasses, underpasses are often utilized to provide continuous and uninterrupted access across a busy thoroughfare or other major obstacles, such as railroads. When designing an underpass it necessary to maintain a vertical clearance of a minimum of 10-feet. Underpasses typically require shorter ramps

and less right-of-way than overpasses. A disadvantage of underpasses is that they can become expensive to construct, especially if the roadway has to be elevated in order to relocate utilities. Also, if the underpass is not well lit, it can create an unsafe environment, discouraging use.



*Underpasses require special design considerations, especially when next to a stream or creek*

When considering the use of underpasses, drainage must be considered, especially if located near a creek or stream. Some of the greenways located within Mecklenburg County utilize underpasses and are often flooded during heavy rain events. Underpasses should be wide enough to for use by multiple users,

and the longer the tunnel, the wider it should be. This provides a sense of security as people are passing by one another. Sufficient lighting should be provided within the underpass to create a safer environment for the user as well.

AASHTO recommends that the minimum width of an underpass be 12-feet. If the underpass structure is longer than 60-feet, a wider width of the underpass is recommended. For short underpasses a vertical clearance of 8-feet is sufficient, however, similar to the width of the underpass, the longer the structure the more vertical clearance that should be provided.

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<sup>1</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pgs. 80-81.

<sup>2</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pg. 82.

<sup>3</sup> **Pedestrian and Bicycle Information Center:**

<http://www.walkinginfo.org/engineering/crossings-enhancements.cfm#curb-extensions>

<sup>4</sup> **Pedestrian and Bicycle Information Center:**

<http://www.walkinginfo.org/engineering/crossings-enhancements.cfm#curb-extensions>

<sup>5</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pg. 83.

<sup>6</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pg. 89.

<sup>7</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pg. 91.

<sup>8</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pg. 61.

<sup>9</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pg. 111

<sup>10</sup> **American Association of State Highway and Transportation Officials (AASHTO).** *Guide for the Planning, Design, and Operation of Pedestrian Facilities.* July 2004, pg. 97.



# CHAPTER 6

## Implementation



## MINT HILL PEDESTRIAN MASTER PLAN





# Implementation

## 6.1 Overview

The goal of this plan is to implement more than just miles of sidewalks and improve intersections within Town limits. Concurrent steps need to be taken to provide for the maintenance of existing and future facilities, their expansion with the Town's growth, and public outreach so that people are aware of the resources at their disposal and how to safely use them.

Identifying the proper tools is essential to the implementation of the Pedestrian Plan. The Town currently requires private developers to include sidewalks as part of their plans. Many of the town's existing pedestrian facilities have been accomplished through this tool; however major gaps in the system still exist, especially along the major thoroughfares. In order to fill in these gaps, the Town will need to utilize additional funding mechanisms, such as grants, to ensure connectivity is achieved town wide. Creating a well connected pedestrian system can be challenging though, especially when right-of-way or property acquisition is required. Therefore, the Town will need to work closely with private property owners to either obtain easements and/or acquire property to fill in those gaps. The Town will also need to work closely with NCDOT. Currently NCDOT owns, operates and maintain almost all the roadways within the Town; therefore the Town needs to work with NCDOT to make sure that pedestrian facilities are included in all new and retrofitted roadways and bridges.

## 6.2 Roles and Responsibilities

The success of this plan cannot be realized without the coordination of several key entities. This section of the chapter will discuss the roles and responsibilities of each and how they will work together to ensure the successful implementation of the Pedestrian Plan. Each of these entities will have a specific role in the implementation of the plan. Figure 6.1 on page 6-3 illustrates of how the coordination between them should occur.

- Board of Commissioners (BOC)
- Planning Board (PB)
- Town Planning and Zoning (T&Z)
- Town Parks and Recreation (P&R)
- Town Public Works (PW)
- Town Police Department (PD)
- Transportation Action Committee
- Developers



- North Carolina Department of Transportation (NCDOT)
- Mecklenburg-Union Metropolitan Planning Organization (MUMPO)
- Local Residents/Advocacy Groups

### **6.2.1 Board of Commissioners**

The Board of Commissioners (BOC) will be responsible for ensuring that improving pedestrian mobility throughout the town remains a priority moving forward. One of the first steps in this process is the adoption of this plan. Through the adoption of this plan, the Town's leadership is recognizing the value of pedestrian transportation and the improvement of the quality of life for the residents. To further the implementation of this plan there is a need for a dedicated transportation staff person who will not only oversee the implementation of this plan, but also the coordination of long-range transportation planning, the capital improvement program, plan reviews, coordination with NCDOT and the Land Use and Environmental Services Agency (LUESA), manage sidewalk projects and the dedication of roadways within the town. The town is growing and will continue to do so. To address the growing needs of Mint Hill in a somewhat strapped budget environment the Town has to leverage all resources to identify, pursue, and secure funding to implement the projects identified in this plan. Many funding opportunities exist at various levels: Federal, State and Regional). To be competitive for these funding opportunities it is imperative that the Town has a dedicated staff person to oversee and manage these tasks. Although various funding opportunities exist with many agencies (see Appendix E for a detailed list) the competition of these resources is intense. Municipalities that have dedicated transportation staff typically do better job of tapping into these resources than the ones that do not. In addition, the BOC should be prepared to do the following:

- Approve ordinance updates and changes that will strengthen pedestrian-related policies.
- Support the expenditure of local funding for the development and maintenance of pedestrian facilities, such as sidewalks, greenways and intersection improvements.
- Support and encourage Town staff to obtain grants and other types of funding for the development of pedestrian facilities.

### **6.2.2 Town Planning Board**

The Town's Planning Board serves as an advisory board to the Board of Commissioners and provides recommendations on planning and zoning related matters. Therefore, the Planning Board will review any policy changes related to

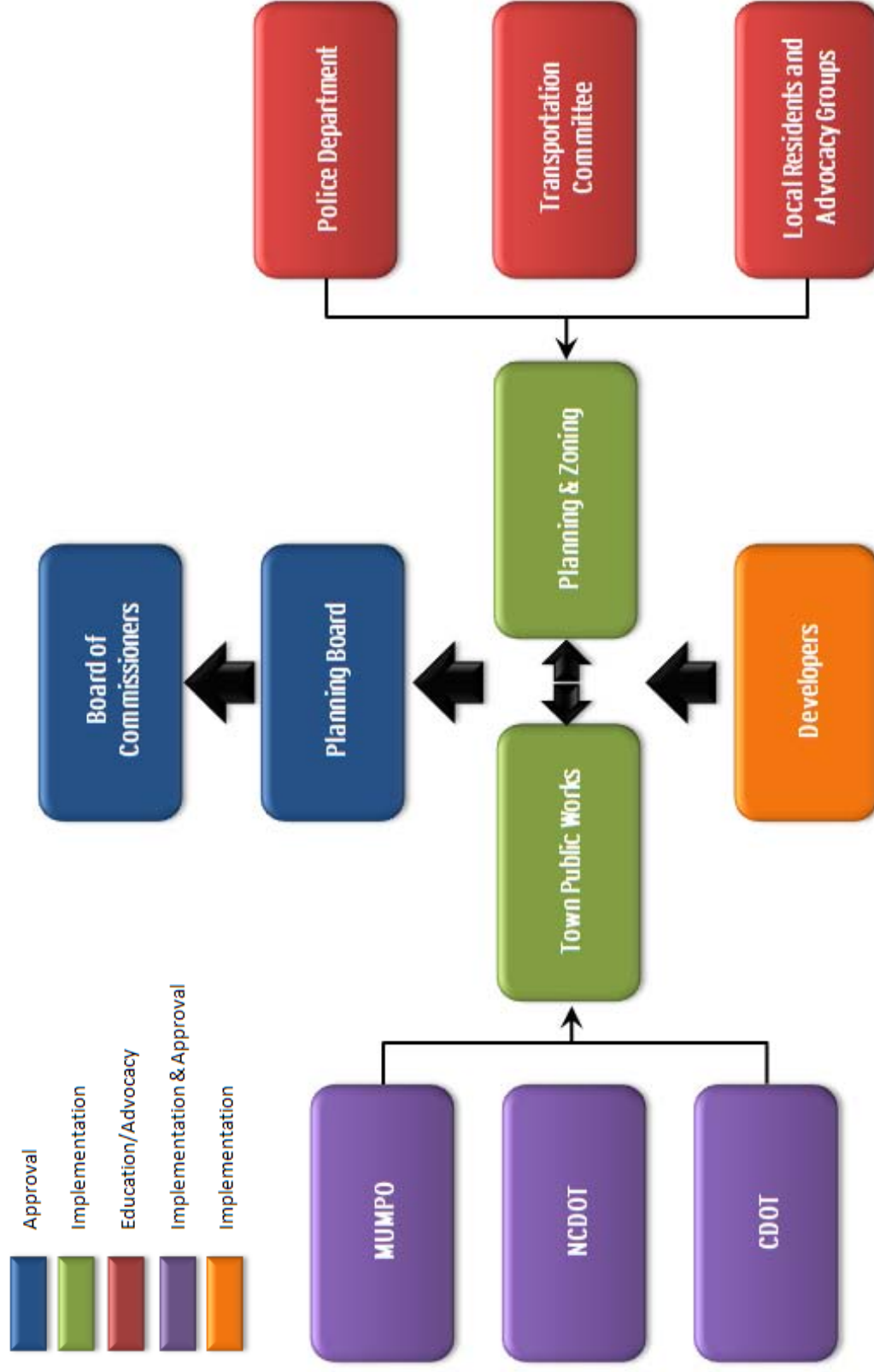
this plan. It will be critical for the Planning Board to become familiar with the Pedestrian Master Plan and be ready to support the recommendations.

### ***6.2.3 Planning and Zoning Department***

The Planning and Zoning Department should be the primary contact for monitoring the implementation of the Pedestrian Plan. They should be involved in the plan review process to ensure that pedestrian facilities are being considered and that the proposed project is consistent with the goals of the Pedestrian Plan. The Planning and Zoning Department should be prepared to do the following:

- Updating and maintaining the GIS database for the pedestrian system.
- Pursue grants and other alternative funding sources to implement the pedestrian infrastructure projects and programs outlined in Chapter 4
- Develop a Pedestrian Safety and Education Program.
- Establish a Transportation Action Committee (TAC)
- Meet with the TAC and provide updates on pedestrian related issues and infrastructure projects. Encourage them to attend public events to assist the Town in promoting pedestrian safety and education
- Coordinate with NCDOT and the Town's Public Works Department to ensure pedestrian facilities are incorporated into new roadway and reconstruction projects
- Continue to coordinate with MUMPO on future multi-modal projects to ensure pedestrian facilities are considered
- Review development plans to ensure the inclusion of pedestrian facilities, such as sidewalks, lighting and signage
- Coordinate with Public Works and Mecklenburg County to ensure pedestrian infrastructure is constructed properly and that pedestrian safety is not being compromised
- Reach out to health organizations and other advocacy groups to assist in promoting walking and healthy lifestyle habits
- Coordinate with the Police Department to develop safety and education programs and information to be shared with citizens
- Coordinate with Mecklenburg County and the Carolina Thread Trail on the development of greenways within Mint Hill
- Present changes/updates to the Town's Unified Development Ordinance
- Work with the BOC and the Town Manager to develop a designated local funding mechanism for pedestrian-related infrastructure

Figure 6.1: Coordination Framework



#### **6.2.4 Parks and Recreation Department**

The Parks and Recreation Department will primarily be responsible for the planning, design and construction of future greenways and parks. This department will need to coordinate closely with the Planning and Zoning Department to ensure that the recommendations from this plan are considered during future greenway efforts. Public outreach and education will be imperative as the Town begins to develop a public greenway system. Main concerns were raised during this process and the Town should work with the community to ensure that all concerns are addressed before constructing any greenway. The Parks and Recreation Department will also need to work closely with the Public Works Department to develop a maintenance program for all future public greenways.

#### **6.2.5 Public Works Department**

The Public Works Department will play a critical role in the implementation of the pedestrian facilities. They currently work with NCDOT to identify capital improvements that are needed within the Town. It will be important for the Public Works Department and NCDOT to become familiar with the recommendations within this plan to ensure that they are included in roadway improvement projects.

The Public Works Department will be responsible for the construction and maintenance of the facilities on town-owned roadways. They will also be responsible for working with the Parks and Recreation Department on the construction and maintenance of any town-owned and maintained greenways. Proper maintenance of these facilities will be critical to the success of the pedestrian program. Currently the Town owns, operates and maintains approximately 100 miles of roadways within the Town limits. As the Town continues to grow and more roadways become their responsibility, it will be important that there are specific roadway maintenance procedures including: repairs, trash removal, mowing and vegetation clearing, edging, and snow and ice removal.

The Public Works Department will need to become familiar with the Standards and Guidelines that are defined in Chapter 5. They should also become familiar with other national standards and guidelines from AASHTO, ITE and NCDOT. Finally, they should coordinate with the Planning and Zoning Department on new construction and reconstruction project to allow for sufficient review time.

#### **6.2.6 Police Department**

The Town's Police Department should work with the Planning and Zoning Department to educate the community pedestrian laws and safe walking habits.

The Police Department is a great resource and the Town should continue to forge a partnership to assist in educating both pedestrians and motorists.

The Police Department should be prepared to:

- Assist the Town in understanding and enforcing pedestrian-related laws in North Carolina
- Enforce all laws (pedestrian and motorist) to increase pedestrian safety. These include, but are not limited to speeding, aggressive driving, running red lights, no turning on red, etc.
- Provide educational opportunities for the Town and citizens
- Provide an opportunity for users to call in and report issues related violations or accident spots. This can be done through an online tool or a hotline.
- Continue to patrol the community to ensure safety of greenways and trails and to work with the Town on identifying opportunities for connectivity.

#### **6.2.7 *Transportation Committee***

It is recommended that the Town create a Transportation Committee that will be responsible for coordinating with the Town to implement the recommendations in this plan. Their support will be critical during the implementation of the plan and they should coordinate and meet with the Planning and Zoning Department to:

- Evaluate the progress of the Plan
- Assist the Town during pedestrian-related events to promote walking and healthy lifestyle characteristics
- Act as the liaison between the Town and the community
- Work with the Planning and Zoning Department to continue to look for opportunities for pedestrian connectivity
- Coordinate with the Transportation Planner to evaluate and assess all transportation related projects within the town

#### **6.2.8 *Developers***

Developers will continue to play an important role in the development of pedestrian facilities. As new development occurs, developers should work closely with the Town and be prepared to do the following:



- Developers should become familiar with the benefits of providing pedestrian amenities, such as sidewalks and greenways.
- Developers should also become familiar with the Standards and Guidelines that are outlined in Chapter 5.
- Finally, developers will participate in the implementation of the plan through compliance with the Unified Development Ordinance, especially Sections 7(f) and (m), Section 13(i), and Section 16(e). They should also become familiar with the Town’s Downtown Overlay Code.

### ***6.2.9 North Carolina Department of Transportation (NCDOT)***

NCDOT owns and operates many major thoroughfares within the town and should continue to work closely with the Town to construct and maintain pedestrian facilities. NCDOT should encourage the Town to adopt the states policy to create “Complete Streets” as their own. In 2009 the NCDOT Board of Transportation approved a Complete Streets policy which according to the policy will “guide existing decision-making and design processes to ensure that all users are routinely considered during the planning, design, construction, funding and operation of North Carolina’s transportation network”. The full description of the policy can be found at the following web link: <http://www.ncdot.org/doh/preconstruct/highway/roadway/policymemos/Design/CompleteStreetsPolicy.pdf>. NCDOT should be prepared to do the following:

- Endorse the Town’s Pedestrian Master Plan and commit to assist in the implementation of the plan, especially on all state maintained roads.
- Work with the Town on future roadway projects, and provide sufficient time for coordination with the Town staff.

The Town should continue to partner with NCDOT to identify issue areas and work on solutions to improve pedestrian mobility and safety. Partnering with them on future projects will not only save the Town from having to fund 100% of the projects cost, it will also strengthen the relationship with NCDOT.

### ***6.2.10 Mecklenburg-Union Metropolitan Planning Organization***

The Mecklenburg-Union Metropolitan Planning Organization (MUMPO) coordinates transportation policy for local governmental jurisdictions within the Charlotte Urbanized Area. They develop transportation plans, such as the 2035 Long-Range Transportation Plan, and programs that assist governing bodies in determining courses of action and in formulating attainable capital improvement programs in anticipation of community needs. They also guide private individuals and groups in planning their decisions which can be important factors in the pattern of future development and redevelopment of the area.<sup>1</sup>

They are responsible for creating the 2035 Long Range Transportation Plan which is a long-term planning document detailing transportation improvements and policies. Transportation improvement projects are not limited to roadways, they include pedestrian facilities such as greenways. The Town should coordinate with MUMPO on future projects to ensure that pedestrian facilities are being included and that the vision of the Pedestrian Master Plan is considered. They should also work with MUMPO and Mecklenburg County Parks and Recreation Department to include greenways in the list of projects within the 2035 Long-Range Transportation Plan.

#### **6.2.11 Local Residents and Advocacy Groups**

The residents of the town and the advocacy groups, such as Fit City Challenge, will play a critical role in the implementation of this plan. The Planning and Zoning Department should work with local advocacy groups and engage them in the education process, especially as it relates to the benefits of an active lifestyles and healthy eating habits. The residents and the advocacy groups should be prepared to do the following:

- Attend BOC and Planning Board meetings where pedestrian related issues are being discussed. It is important to illustrate to the BOC and the Planning Board that pedestrian safety and improvements are a priority to the community.
- Volunteer to assist at pedestrian related events and gather support amongst friends, family and neighbors.
- Continue to look for opportunities for partnerships and improving pedestrian connectivity.

### **6.3 Program Monitoring**

As the Town begins to implement this Plan it will be critical to monitor its progress. Therefore, it is recommended that the Town develop a system of measuring the performance of the program. These performance measures should be based on safety, usage, education, and level of completion. Data that can be easily accessed and distributed should be used to develop the performance measures. For example, pedestrian safety can be measured by the amount of pedestrian crashes that are recorded by law enforcement. That is why it will be extremely important to ensure that law enforcement is properly recording these incidences, so that the Town can measure the success of certain pedestrian improvements.

The Town should also re-examine and re-prioritize the project listing based on opportunities that may arise, such as donation of land for greenways and parks or proposed development projects.

## **6.4 Facility Development**

The Town should be involved in the planning and design of any new or reconstructed roadway or bridge to ensure that pedestrian facilities are incorporated. There are several ways that the Town can implement the proposed projects in the Pedestrian Plan including: utilizing roadway and bridge construction and reconstruction projects, retrofitting existing roadways with new pedestrian facilities, and the NCDOT TIP process that was mentioned earlier.

## **6.5 Roadway/Bridge Construction & Reconstruction**

The Town should ensure that pedestrian facilities are included as part of any new or reconstructed roadway, bridge, and underpass. All roadway projects should reference the Town's Comprehensive Plan for the appropriate sidewalk width. NCDOT bridge policy states that all new or reconstructed NCDOT bridges shall incorporate pedestrian facilities on both sides of the bridge. The sidewalks should be a minimum of 5-feet with 42-inch railings on the outside to protect the pedestrian from falling off the bridge.

Roadway projects that require the installation or modification of culverts for streams and creeks should provide sufficient room for pedestrian access through the culvert, especially if greenways or pedestrian paths are planned for the area.

## **6.6 Operation and Maintenance**

Proper maintenance of the pedestrian facilities is essential to the sustainability of the pedestrian system. If the facilities are not maintained properly they will fall into disrepair and pedestrians won't be able to use the facilities. During site visits and the walking audit, participants found signs of disrepair and lack of maintenance, such as weeds and grass that were overtaking some of the sidewalks and trip hazards along many of the sidewalks. Over time, this can reduce the amount of usable space on the sidewalk.

The operation and maintenance of the pedestrian system will be a collaborative effort of various departments and organizations including, but not limited to, the Town's Planning and Zoning Department, Public Works, the Park and Recreation Department, the Police Department and NCDOT. The success of the Pedestrian Plan will rely on the ability of these organizations and departments to cohesively work together on a daily basis.

Additionally, there should be a system in place that allows users to provide suggestions and feedback regarding maintenance issues. Part of that system should include for a timely response to the user. The Pedestrian Plan web page

mentioned earlier in the report would be an ideal location for a user feedback form and maintenance request form.

## 6.7 Prioritization of Pedestrian Projects

The Pedestrian System Plan Map (Figure 3.1) that is located in Chapter 3 illustrates all of the proposed pedestrian projects for this plan. There are over 120 sidewalk projects, 21 greenway projects and 18 intersection improvement projects. Realistically, the Town cannot build all of these projects at once due to several factors; mostly funding and land ownership. Therefore it is imperative that each project be assigned a realistic timeframe for implementation. Providing a timeframe for each project gives residents, developers, and adjacent communities an understanding of when the improvements will likely occur. It also provides the BOC, the Planning Board and Town staff with guidance on budgeting for capital projects, the formulation of work plans, and the pursuit of grants. To develop this time frame each project was placed into a Project Matrix (see Appendix B). The Project Matrix was then used to apply a series of criteria that divide the projects into short-term, mid-term and long-term priorities (see Table 6.1 below).

**Table 6.1: Prioritization Criteria**

Criteria	Point Value
Downtown Overlay Proximity (within Downtown Overlay Only)	3
Existing School Proximity (1/2 mile radius )	3
Connect to Existing Infrastructure	3
Existing Park Proximity (1/2 mile radius)	3
Existing Transit Stop Proximity (1/2 mile radius)	2
Existing Major Retail/Commercial Proximity (1/2 mile radius)	2
Existing Non-Retail Major Employment Proximity (1/2 mile radius)	2
Existing Mixed-Use Proximity (1/2 mile radius)	2
Future School Proximity (1/2 mile radius)	1
Future Park Proximity (1/2 mile radius)	1
Future/Proposed Greenway Proximity (1/2 mile radius)	1
<b>Total Possible Raw Score</b>	<b>23</b>

Based on the criteria, each project scored a specific amount of points based on its proximity to the type of destination. Some criteria were given a higher point value based on its importance to the pedestrian network and input received

from the Steering Committee, Town staff and the public. The total raw score that any project could receive was 23. In order to simplify the process and create a 100 point system, each raw score was normalized by a factor of 4.35.

Short-term projects are those that scored the highest (67-100) based on the Prioritization Criteria (see Tables 6.1). The Town should strive to implement these projects within the next 1-5 years. Mid-term projects had the next highest score (34-66) and should be completed in the next 5-10 years. Finally, long-term project are those that had the lowest scores (0-33) and should be implemented over the next 10-20 years. Table 6.2 lists the prioritization categories and the point value ranges for each.

**Table 6.2: Priority Categories**

Priority	Point Range
Short-Term Priority Projects (1-5 years)	67 – 100
Mid-Term Priority Projects (5-10 years)	34 – 66
Long-Term Priority Projects (10-20 years)	0 – 33

### **6.7.1 Top Rated Projects**

Based on the prioritization process described above, a list of top rated projects has been developed to allow Town staff to monitor the progress of their implementation. Tables 6.3-6.5 provide a list of those top rated projects and are separated by facility type: Proposed Greenways, Proposed Intersection Improvement Areas and Proposed Sidewalks. The top rated intersection projects also includes the mid-block crossing projects that were identified during the process. The projects are listed in order of how they rated based on the criteria that was described above. It should be noted to that during the final public meeting, participants were asked to rank these top rated projects based on their preference. The Town should take this information into consideration when making decisions about future pedestrian improvements.

The prioritized list of project provides the Town with a foundation to begin making decisions on pedestrian improvements. This list is merely a foundation and just because a project scored high based on the criteria and is listed as a Short-Term priority, doesn't necessarily mean that it will be the first project built.



There will be many other factors that will need to be considered to decide when a project gets built. Some of those factors include, but are not limited to:

- Funding
- Land ownership
- Community Input
- Scheduled Roadway Improvements
- Private Development

**Table 6.3: Top Rated Greenway Projects**

Project ID	From	To	Local Plan	Creek Name	Length (mi)	Scheduled Priority
1	Idlewild Rd	NC 51 / Blair Rd	Mecklenburg County Greenways	Irvin Creek	3.53	Short-Term
15	SE of NC 51 (downtown overlay)	North of I-485	Comprehensive Land Use Plan	Stevens Creek Tributary	1.99	Short-Term
5	Proposed greenway (west of Lawyers Rd)	Chicopee Dr	Comprehensive Transportation Plan	Irvin Creek	0.63	Short-Term
16	NC 51/Lawyers Rd	Lawyers Rd/Heath Lake Dr	Comprehensive Land Use Plan	Goose Creek Tributary	1.31	Short-Term
14	NC 51/Windy Knoll Ln	N of NC 218/NC 51 Intersection	Comprehensive Land Use Plan	NA	1.83	Mid-Term

**Table 6.4: Top Rated Intersection Projects**

Project Location	Intersection Type	Scheduled Priority
NC 51 / NC 218	Signalized	Short-Term
Lawyers Road / Lebanon Road / Wilson Grove Road	Signalized	Mid-Term
NC 51 / Idlewild Road	Signalized	Mid-Term
NC 51 / Bain School Road	Non-Signalized (mid-block)	Short-Term
NC 51 / Phylliss Lane	Non-Signalized (mid-block)	Mid-Term

**Table 6.5: Top Rated Sidewalk Projects**

Project ID	Project Location	From	To	Side of Street	Length (mi)	Scheduled Priority
13	NC 51	Phylliss Ln	Mint Hill Family Practice	South	0.14	Short-Term
14	NC 51	Hawthorne Dr	North of Evans Rd (Showmars)	Southeast	0.25	Short-Term
15	NC 51	North of Evans Rd (Wendy's)	Dunkin' Donuts	Northwest	0.05	Short-Term
16	NC 51	Southeast side of Dunkin' Donuts	Lawyers Road	Northwest	0.04	Short-Term
17	NC 51	Warehouse Rd	West side of Hawthorne's Pizza	North	0.07	Short-Term
18	NC 51	East side of Hawthorne's Pizza	Mint Hill Village Ln	North	0.03	Short-Term
19	NC 51	East side of Mint Hill Village	Southwest of Sunset Dr	Northeast	0.29	Short-Term
41	Lawyers Rd	Truelight Church Rd	North of NC 51	East	0.58	Short-Term
42	Lawyers Rd	NC 51	North of Hollow Oak Dr	West	0.46	Short-Term
43	Lawyers Rd	Nelson Rd	North of NC 51	West	0.28	Short-Term
44	Lawyers Rd	South of NC 51	North side of BB&T	East	0.02	Short-Term
46	Lawyers Rd	South side of BB&T	Shelbourne Pl	East	0.63	Short-Term
82	Bain School Rd	NC 51	Philadelphia Church Rd West	North	0.20	Short-Term
58	Wilgrove Mint Hill Rd	Pine Hill Rd	South of Nelson Rd	West	0.10	Mid-Term
59	Wilgrove Mint Hill Rd	Pine Hill Rd	NC 51	East	0.39	Mid-Term
63	NC 218	NC 51	Jefferson Colony Rd	North	0.76	Mid-Term
11	NC 51	Zeb Morris Wy	Robin Hollow Dr	East	1.35	Mid-Term
12	NC 51	North side of Budget Self Storage	Carving Tree Dr	West	1.02	Mid-Term
39	Lawyers Rd	Lebanon Rd	Truelight Church Rd	South	1.53	Mid-Term
40	Lawyers Rd	Wilson Grove Rd	Truelight Church Rd	North	1.52	Mid-Term

## 6.8 Action Item Matrix

In order to begin implementation of the Pedestrian Master Plan, the Town will need to complete several action items. The Action Item Matrix provides a series of action steps or tasks associated with the implementation of this plan. These action steps have been organized by the following categories:

- Planning
- Funding
- Development/Construction
- Maintenance
- Education
- Coordination

Under each category are tasks that are critical to the implementation of the recommendations in this plan. These tasks have been broken down into scheduled priorities based on the following:

- Immediate (1-3 years)
- Short-Term (3-5 years)
- Mid-Term (5-10 years)
- Long-Term (over 10 years), and
- On-Going.

In addition to assigning timeframes to the tasks, the Action Item Matrix provides guidance for the Lead agency/entity and the Supporting agency/entity. This matrix should be used to track the completion of each task. The Action Item Matrix can be found in Appendix C.

### 6.8.1 *Priority Action Items*

In order to begin implementing the recommendations outlined in this report, Mint Hill will need to take immediate action following the adoption of this plan. This section provides a list of actions that the Town should take to begin implementing policy recommendations and acquiring funding for pedestrian programs and infrastructure improvements. Table 6.6 provides a list of Priority Action Items for each of the categories listed above.

**Table 6.6: Priority Action Items**

Action Item #	Action Item
<b>Planning</b>	
P.1	Present Plan recommendations to the Planning Board for Endorsement
P.2	Present Plan recommendations to the BOC for adoption
P.3	Adopt the Pedestrian Master Plan
P.4	Incorporate Standards and Guidelines (Chapter 5) into the Town's Development Standards
<b>Funding</b>	
F.1	Pursue Safe Routes to School grants for Infrastructure, Non-Infrastructure and Action Plans
F.2	Develop a long-term funding strategy for the development of pedestrian facilities. The Town should begin earmarking local funds specifically for pedestrian infrastructure improvements.
<b>Development/Construction</b>	
D.1	Finish sidewalks from Brighton Park to the Square
<b>Maintenance</b>	
M.1	Incorporate pedestrian maintenance into the Town's maintenance program
<b>Education</b>	
E.1	Develop communication program to educate the community about the Pedestrian Master Plan
<b>Coordination</b>	
C.1	Initiate communication with surrounding municipalities to discuss pedestrian related issues and to coordinate on adjacent pedestrian projects
C.2	Initiate discussions with local and regional health organizations to educate community about benefits of walking

<sup>1</sup> Mecklenburg-Union Metropolitan Planning Organization:  
[http://www.mumpo.org/About\\_Us.htm](http://www.mumpo.org/About_Us.htm)



## MINT HILL PEDESTRIAN MASTER PLAN



# **APPENDICES**

**Appendix A: Questionnaire Results**

**Appendix B: Pedestrian Project Matrix**

**Appendix C: Action Item Matrix**

**Appendix D: Terminology**

**Appendix E: Funding**



## MINT HILL PEDESTRIAN MASTER PLAN





# **Appendix A**

## **Questionnaire Results**







## **MINT HILL PEDESTRIAN MASTER PLAN**

### 1. Are you a resident of Mint Hill?

		Response Percent	Response Count
Yes		94.4%	117
No		5.6%	7
If yes, which neighborhood do you live in?			111
answered question			124
skipped question			0

### 2. How often do you and/or your family walk?





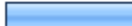







		Response Percent	Response Count
Daily		63.2%	74
Weekly		28.2%	33
Monthly		6.0%	7
Not at all		2.6%	3
Other (please specify)			10
answered question			117
skipped question			7

3. If you walk, what is the reason? (check all that apply)			
		Response Percent	Response Count
Exercise	<div><div></div></div>	90.8%	108
Transportation	<div><div></div></div>	14.3%	17
Errands	<div><div></div></div>	34.5%	41
Recreational/Leisure	<div><div></div></div>	72.3%	86
Other (please specify)			21
answered question			119
skipped question			5

4. If it were easy to walk around Mint Hill, where would you and/or your family walk to? (check all that apply)			
		Response Percent	Response Count
Coffee shop	<div><div></div></div>	74.8%	86
Grocery store	<div><div></div></div>	80.0%	92
Bookstore	<div><div></div></div>	53.9%	62
Park or Greenway	<div><div></div></div>	89.6%	103
School	<div><div></div></div>	24.3%	28
Church	<div><div></div></div>	22.6%	26
Other (please specify)			43
answered question			115
skipped question			9








5. Of the following improvements, which ones would most encourage you to increase your walking? (check all that apply?)

		Response Percent	Response Count
Better crosswalks (raised, stamped asphalt, signals, striped)		33.6%	41
More sidewalks		91.0%	111
Wider sidewalks (wider than 5-feet)		13.9%	17
Longer 'Walk' signals at intersections		10.7%	13
Pedestrian Refuges at major road crossings		22.1%	27
Off-road trails and/or greenways)		60.7%	74
Better lighting along pedestrian routes		36.9%	45
More pedestrian connections between businesses and neighborhoods		62.3%	76
Mid-block crossings		13.1%	16
Better handicap facilities (ramps)		4.1%	5
Landscaping along the roadway or trail (trees, flowers, etc)		28.7%	35
Street furniture (benches, water fountains, etc.)		37.7%	46
Other (please specify)			14
answered question			122
skipped question			2

6. Name some of the best places you can get to now on foot in Mint Hill.		
		Response Count
		102
	answered question	102
	skipped question	22

7. Do you know of any areas where people have worn paths or formed pedestrian connections that the Town could improve with sidewalks or paved trails for pedestrian use?		
		Response Count
		71
	answered question	71
	skipped question	53

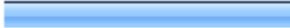
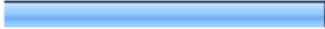
8. Are there any particular pedestrian improvements that you would like to see the Town make?		
		Response Count
		88
	answered question	88
	skipped question	36

9. How satisfied are you with our current pedestrian infrastructure?			
		Response Percent	Response Count
Very satisfied		5.7%	7
Somewhat satisfied		36.9%	45
Somewhat dissatisfied		23.0%	28
Not Satisfied		28.7%	35
Neutral		5.7%	7
Comment			24
answered question			122
skipped question			2

10. What do you see as the Town's greatest STRENGTH in terms of walkability?		Response Count
		78
answered question		78
skipped question		46

11. What do you see as the Town's greatest WEAKNESS in terms of walkability?		Response Count
		81
answered question		81
skipped question		43

## 12. Do you feel that the Town's pedestrian infrastructure promotes safety?

		Response Percent	Response Count
Yes		47.2%	51
No		52.8%	57
Other (please specify)			38
answered question			108
skipped question			16

## 13. Any additional comments?

	Response Count
	47
answered question	47
skipped question	77

## 1. Are you a resident of Mint Hill?

If yes, which neighborhood do you live in?		
1	ETJ	Jul 14, 2010 2:02 PM
2	Farmwood	Jul 15, 2010 3:49 PM
3	Farmwood	Jul 15, 2010 5:02 PM
4	Glencroft	Jul 16, 2010 12:27 PM
5	danbrook park	Jul 16, 2010 4:48 PM
6	St. Ives	Jul 16, 2010 9:09 PM
7	Bainbridge	Jul 17, 2010 1:37 PM
8	Heath Glen	Jul 17, 2010 2:15 PM
9	Kemper Downs	Jul 17, 2010 2:29 PM
10	Farmwood	Jul 17, 2010 4:21 PM
11	Versage	Jul 17, 2010 5:57 PM
12	Brighton Park	Jul 17, 2010 7:13 PM
13	Brighton Park	Jul 17, 2010 11:42 PM
14	Brighton Park	Jul 18, 2010 3:25 PM
15	Brighton Park	Jul 18, 2010 5:35 PM

## 1. Are you a resident of Mint Hill?

If yes, which neighborhood do you live in?		
16	Oxfordshire	Jul 18, 2010 11:26 PM
17	Hidden Forest	Jul 19, 2010 4:53 PM
18	Farm Wood	Jul 19, 2010 7:48 PM
19	Ellington Farm	Jul 20, 2010 6:48 PM
20	Jonathans Landing	Jul 21, 2010 8:16 PM
21	Brighton Park	Jul 23, 2010 8:07 PM
22	Versage	Jul 23, 2010 8:59 PM
23	Hidden Hills	Jul 24, 2010 10:23 PM
24	Farmwood East	Jul 27, 2010 4:46 PM
25	Large Oak	Jul 29, 2010 1:44 PM
26	Lawyers Station	Aug 3, 2010 3:09 PM
27	n/a	Aug 4, 2010 8:55 PM
28	Brighton Park	Aug 11, 2010 4:47 PM
29	Meadows of mint hill	Aug 11, 2010 5:04 PM
30	Meadows of Mint Hill	Aug 11, 2010 10:26 PM
31	Brighton Park	Aug 12, 2010 9:43 PM
32	Brighton Park	Aug 12, 2010 10:37 PM
33	Brighton Park	Aug 12, 2010 10:40 PM
34	brighton park	Aug 12, 2010 10:49 PM
35	Brighton Park	Aug 12, 2010 10:56 PM
36	Brighton Park	Aug 13, 2010 12:23 AM
37	Brighton Park	Aug 13, 2010 12:48 AM
38	Brighton Park	Aug 13, 2010 12:54 AM
39	Brighton Park	Aug 13, 2010 12:59 AM
40	meadows of mint hill	Aug 13, 2010 1:27 AM
41	Brighton Park	Aug 13, 2010 1:35 AM
42	Brighton Park	Aug 13, 2010 2:02 AM
43	Brighton Park	Aug 13, 2010 2:59 AM
44	Brighton Park	Aug 13, 2010 3:02 AM
45	Brighton Park	Aug 13, 2010 4:42 AM
46	Brighton Park	Aug 13, 2010 11:58 AM
47	brighton park	Aug 13, 2010 12:14 PM
48	Ashbourne Lane, a single (no through) street off Lawyer's Road (across from Farmwood East)	Aug 13, 2010 12:20 PM
49	Brighton Park	Aug 13, 2010 3:18 PM
50	Brighton Park	Aug 13, 2010 3:26 PM
51	Brighton Park	Aug 13, 2010 4:14 PM
52	Brighton Park	Aug 13, 2010 4:36 PM
53	Brighton Park	Aug 13, 2010 5:16 PM
54	Brighton Park	Aug 13, 2010 8:42 PM
55	Brighton Park	Aug 14, 2010 1:45 AM
56	Brighton Park	Aug 14, 2010 11:43 PM
57	brighton park	Aug 15, 2010 2:41 AM

## 1. Are you a resident of Mint Hill?

If yes, which neighborhood do you live in?		
58	Brighton Park	Aug 16, 2010 12:31 AM
59	brighton park	Aug 16, 2010 4:16 PM
60	Mint Lake Village	Aug 16, 2010 7:37 PM
61	Jonathan's Landing	Aug 17, 2010 12:49 AM
62	brighton park	Aug 17, 2010 6:14 PM
63	Danbrooke Park	Aug 18, 2010 2:34 PM
64	Farmwood / Singletree Rd	Aug 18, 2010 6:09 PM
65	Farmwood, Apple Creek Entrance	Aug 19, 2010 2:21 PM
66	Mint Lake Village	Aug 20, 2010 12:48 PM
67	Brighton Park	Aug 20, 2010 1:05 PM
68	Mint Lake Village	Aug 20, 2010 1:28 PM
69	off of Margaret Wallace	Aug 21, 2010 8:23 PM
70	Heathfield	Aug 23, 2010 9:52 PM
71	Olde Sycamore	Aug 25, 2010 7:04 PM
72	Lawyers Road near the Square	Aug 26, 2010 2:15 PM
73	Hidden Hills	Aug 26, 2010 7:02 PM
74	Fairington Oaks	Aug 26, 2010 7:42 PM
75	Oxfordshire	Aug 26, 2010 7:47 PM
76	None	Aug 26, 2010 7:53 PM
77	Shea Summerwood Development,off Bartlett	Aug 26, 2010 8:18 PM
78	Oaks at Oxforshire	Aug 26, 2010 8:21 PM
79	Oxfordshire	Aug 26, 2010 8:21 PM
80	Ashe Plantation	Aug 26, 2010 9:34 PM
81	none - on Thompson Rd	Aug 27, 2010 12:59 AM
82	Farmwood Apple Creek	Aug 27, 2010 1:18 AM
83	Oxfordshire	Aug 27, 2010 1:39 AM
84	windemere	Aug 27, 2010 3:34 AM
85	Lawyers Station	Aug 27, 2010 4:18 AM
86	Farmwood	Aug 27, 2010 6:17 AM
87	oxfordshire	Aug 27, 2010 11:52 AM
88	Bainbridge	Aug 27, 2010 12:03 PM
89	Brighton Park	Aug 27, 2010 12:27 PM
90	Tarawoods	Aug 27, 2010 1:05 PM
91	truelight church road	Aug 29, 2010 1:05 PM
92	versage	Aug 29, 2010 3:45 PM
93	Annette Avenue	Aug 30, 2010 2:39 AM
94	rural	Aug 30, 2010 1:43 PM
95	mint lake village	Aug 30, 2010 8:26 PM
96	Farmwood	Aug 31, 2010 7:34 PM
97	Brighton Park	Sep 1, 2010 1:24 PM
98	Brighton Park	Sep 1, 2010 1:57 PM
99	Heathfield	Sep 1, 2010 3:13 PM
100	Heath Field off Lawyers Road	Sep 2, 2010 12:46 AM



## 1. Are you a resident of Mint Hill?

If yes, which neighborhood do you live in?		
101	Ellington Farm	Sep 6, 2010 9:12 PM
102	Heathergate	Sep 10, 2010 12:58 PM
103	Glencroft	Sep 13, 2010 12:35 PM
104	n/a	Sep 23, 2010 5:48 PM
105	Farmwood	Nov 4, 2010 4:41 PM
106	Fairington Oaks	Nov 19, 2010 1:36 AM
107	Ellington Farm	Nov 19, 2010 1:49 PM
108	Farmwood	Dec 1, 2010 12:30 AM
109	Farmwood, on Singletree Road	Dec 1, 2010 7:46 PM
110	Ashe Plantation	Dec 2, 2010 12:37 AM
111	Farmwood East	Dec 5, 2010 4:12 PM

## 2. How often do you and/or your family walk?

Other (please specify)		
1	we would walk daily if there were any sidewalks	Jul 16, 2010 4:48 PM
2	On occasion	Jul 23, 2010 8:59 PM
3	occasionally	Aug 4, 2010 8:55 PM
4	3 to 4 days per week	Aug 13, 2010 1:27 AM
5	From time to time	Aug 17, 2010 12:49 AM
6	Bicycle ride	Aug 18, 2010 2:34 PM
7	Occasionally	Aug 26, 2010 7:42 PM
8	I bicycle around town a couple of times a week.	Aug 26, 2010 8:21 PM
9	occasionally	Aug 27, 2010 12:59 AM
10	once in awhile	Aug 29, 2010 2:13 AM

## 3. If you walk, what is the reason? (check all that apply)

Other (please specify)		
1	walk the dog	Jul 16, 2010 4:48 PM
2	Weight loss and cardio vascular	Jul 17, 2010 2:29 PM
3	To get to the bus stop.	Jul 17, 2010 7:13 PM
4	Walking dog	Jul 17, 2010 11:42 PM
5	To walk the dog	Jul 19, 2010 4:53 PM
6	Food/Restaurants	Aug 13, 2010 4:42 AM
7	Get to know my neighbors and the neighborhood.	Aug 13, 2010 3:26 PM
8	exploration, observation, security	Aug 13, 2010 4:14 PM
9	Dog	Aug 14, 2010 11:43 PM
10	Save gas money!	Aug 16, 2010 12:31 AM

### 3. If you walk, what is the reason? (check all that apply)

Other (please specify)		
11	Members of household from time to time utilize CATs	Aug 23, 2010 9:52 PM
12	We have two English mastiffs	Aug 26, 2010 7:23 PM
13	dog	Aug 26, 2010 7:47 PM
14	I also bicycle for transportation.	Aug 26, 2010 8:21 PM
15	visit friends	Aug 27, 2010 12:59 AM
16	Walk dog.	Aug 27, 2010 1:39 AM
17	Rollout garbage, pick up mail & newspapers	Aug 27, 2010 6:17 AM
18	employment	Aug 30, 2010 1:43 PM
19	pets	Aug 31, 2010 7:34 PM
20	With more access outside of our community we would walk more for transportation.	Sep 1, 2010 3:13 PM
21	To enjoy the many restaurants in our downtown area.	Sep 13, 2010 12:35 PM

### 4. If it were easy to walk around Mint Hill, where would you and/or your family

Other (please specify)		
1	My office	Jul 15, 2010 6:01 PM
2	restaurants	Jul 16, 2010 9:09 PM
3	Downtown restaurants & stores	Jul 17, 2010 2:15 PM
4	Up route 51 toward Wilgrove Mint Hill Road and back	Jul 17, 2010 2:29 PM
5	Bus Stop	Jul 17, 2010 7:13 PM
6	Town Hall, Carolina Creamery	Jul 17, 2010 11:42 PM
7	Veterinarian's office, drug store, bookstore - if there was one, restaurants	Jul 19, 2010 4:53 PM
8	restaurants, mail box	Jul 20, 2010 6:48 PM
9	We walk to the library, some businesses, friends and the park	Aug 11, 2010 4:47 PM
10	Ice cream	Aug 11, 2010 5:04 PM
11	I'd love to walk to Hawthorne's from Brighton Park while staying on a sidewalk the whole way.	Aug 12, 2010 9:43 PM
12	I now walk to my dentist and to the library; would love to walk to the bank (SECU).	Aug 12, 2010 10:37 PM
13	restaurants, library	Aug 12, 2010 10:56 PM
14	Recreation areas	Aug 13, 2010 12:23 AM
15	Library, Drug Store, Park, Ice Cream Shop, Cottage Knits and other neighborhoods	Aug 13, 2010 3:02 AM
16	Restaurants	Aug 13, 2010 4:42 AM
17	All of town center is within walking distance, but there is no sidewalk from our street to 51/Matthews-Mint Hill Rd. If there was we would walk to all events there (May concerts, June movies, Mint Hill Madness, etc.)	Aug 13, 2010 12:20 PM
18	Would love to walk to different restaurants.	Aug 13, 2010 3:26 PM
19	library, restaurants, town hall, bank, shops, museum, arts center, farm market, MH Madness activities	Aug 13, 2010 4:14 PM
20	Library	Aug 14, 2010 1:45 AM
21	All of the above	Aug 14, 2010 11:43 PM

#### 4. If it were easy to walk around Mint Hill, where would you and/or your family

Other (please specify)		
22	matthews mint hill 51 its very hard to go to the ice cream shop or dunkin donuts. no sidewalks on lawyers or park of 51 its dangerous	Aug 15, 2010 2:41 AM
23	I walk to the dentist, library , restaurants, bank and stores.	Aug 16, 2010 12:31 AM
24	Restaurants	Aug 16, 2010 7:37 PM
25	Friend's house	Aug 17, 2010 12:49 AM
26	Library	Aug 19, 2010 2:21 PM
27	up and down hwy 51 from Peggy Rd to Wilgrove Mint Hill Rd.	Aug 20, 2010 1:05 PM
28	just walk with no particular destination	Aug 21, 2010 8:23 PM
29	Hardware, other local shops	Aug 23, 2010 9:52 PM
30	Would like to have a place TO walk to from our neighborhood.	Aug 25, 2010 7:04 PM
31	From Lawyers Rd. and 51 towards Charlotte to the road going to Brighton Park & Lawyers Rd. & 51 to Lowe's Supermarket.	Aug 26, 2010 2:15 PM
32	library	Aug 26, 2010 7:47 PM
33	And easier and quicker to get to all of these via bicycle, too. And safer.	Aug 26, 2010 8:21 PM
34	friends' houses, everywhere	Aug 27, 2010 12:59 AM
35	Lebanon Road Bridge if it is ever completed	Aug 27, 2010 6:17 AM
36	Library	Aug 27, 2010 12:03 PM
37	around the block	Aug 29, 2010 2:13 AM
38	dollar tree, subway	Aug 30, 2010 8:26 PM
39	To the Downtown Area restaurants & stores	Sep 2, 2010 12:46 AM
40	Restaurants	Nov 19, 2010 1:36 AM
41	None of the above. I prefer to walk in my neighborhood	Dec 1, 2010 12:30 AM
42	NOT GREENWAYS along Irvin Creek. Wildlife will be disturbed which I will be actively protest. Water drainage polluting the creek and Deer wildlife will be disturbed.	Dec 1, 2010 7:46 PM
43	restaurants, bank, library, town hall, dry cleaner, hair salon, nail salon, hardware store,	Dec 5, 2010 4:12 PM

#### 5. Of the following improvements, which ones would most encourage you to

Other (please specify)		
1	To continue the sidewalks on route 51 and connect them to those in Brighton Park, which Matthews has started, into Mint Hill and down 51 toward the entrance to the new high school would make for a good distance to walk.	Jul 17, 2010 2:29 PM
2	crosswalks - striped - absolutely not raised.	Jul 20, 2010 6:48 PM
3	Compact developments and mixture of land uses. Maximize parking lot size and locate them behind buildings	Jul 23, 2010 8:07 PM
4	Intersections present the biggest problem downtown along with unconnected side walks. It would seem, with the focus on downtown, that sidewalks would support safely getting to local businesses, schools and parks.	Aug 11, 2010 4:47 PM
5	The crossing at 51 and Lawyers never seems to work so after waiting through several rotations of the lights I finally have to cross without the pedestrian signal giving the okay.	Aug 12, 2010 9:43 PM
6	More businesses- drive-throughs should be allowed	Aug 13, 2010 2:02 AM

## 5. Of the following improvements, which ones would most encourage you to

Other (please specify)		
7	pedestrian crossing near the library	Aug 13, 2010 2:59 AM
8	Need crosswalk marking or something to make crossing from Brighton Park to the Library easier for pedestrians. It is not a safe crossing now.	Aug 13, 2010 3:26 PM
9	for citizen safety, immediate attention for cross walks on Hwy 51 now needed at: a) Library & Brighton Park; b) Medical center/ pharmacy & Brighton Park; c) Queens Grant School & Brighton Park Crossings desperately needed also at: Hwy 51 & Hwy 218; Nelson Rd. & Evans Rd.; Drs. Museum & Bain School Rd. (across Hwy 51); and Truelight Church Rd. & Lawyers Glen Retirement Community	Aug 13, 2010 4:14 PM
10	Layer,s rd and 51 no side walks fr brighton pk walking out of the plza going to townhall passing wendy,s need side walks thank you	Aug 17, 2010 6:14 PM
11	Along Lawyers Rd from Lebanon to 51 and Bike Pathm, too	Aug 19, 2010 2:21 PM
12	Walking clubs/organizations advertised, started!	Aug 25, 2010 7:04 PM
13	Sidewalks on both sides of the street so you don't have to cross streets several times to get where you're going.	Aug 26, 2010 2:15 PM
14	No greenways in residential areas!!!!!! I don't want strangers walking through my backyard at any time of the day or night. Nor do I want to give up use of large portions of my backyard. It is also a security issue-easy access to the back of my house by strangers and potential criminals is a very serious detriment	Dec 1, 2010 12:30 AM

## 6. Name some of the best places you can get to now on foot in Mint Hill.

Response Text		
1	None	Jul 14, 2010 2:02 PM
2	Brighton Park, Mint Hill Festival Shopping Center, Town Hall	Jul 15, 2010 3:27 PM
3	Brightmore area, Mr. Inspection area at crossroads.	Jul 15, 2010 5:02 PM
4	All 4 corners at Lawyers Road & Pineville Matthews	Jul 15, 2010 6:01 PM
5	Uptown shopping and the library.	Jul 16, 2010 12:27 PM
6	no where without dodging cars	Jul 16, 2010 4:48 PM
7	Jimmies, Lowes, the Brighton Park trail	Jul 16, 2010 9:09 PM
8	I can only walk around my neighborhood right now. I don't have any connections to other establishments yet.	Jul 17, 2010 1:37 PM
9	None because there are no sidewalks from our neighborhood into downtown Mint Hill	Jul 17, 2010 2:15 PM
10	Few if any from Kemper Lane. I was walking on 51 until someone riding in a dump truck opened the right door and tried to take my head off but no more.	Jul 17, 2010 2:29 PM
11	Downtown Mint Hill	Jul 17, 2010 4:21 PM
12	Library, Lowe's Foods	Jul 17, 2010 5:57 PM
13	Lowe's Foods Monroe Hardware	Jul 17, 2010 7:13 PM
14	Lowes Food, Don Juan Mexican Restaurant, Jimmy's Restaurant, ABC Liquor store, StoneHill BBQ Restaurant	Jul 17, 2010 11:42 PM



## 6. Name some of the best places you can get to now on foot in Mint Hill.

	Response Text	
15	*Walk my daughter to Daycare *Grocery store *Farmers Market *Restaurants *Park on Wilgrove/Fairview	Jul 18, 2010 3:25 PM
16	Grocery store (Lowe's), Mint Hill Library, Big Guys Pizza, Mint Hill Barber Shop, Town Hall, Mint Hill Arts, Mint Hill Family Practice, Jimmies	Jul 18, 2010 5:35 PM
17	Mint Hill Park on Wilgrove-Mint Hill Rd	Jul 18, 2010 11:26 PM
18	There are none because we are prisoners in the neighborhood due to lack of sidewalks. Won't walk on road shoulders.	Jul 19, 2010 4:53 PM
19	neighborhood only at this time	Jul 19, 2010 7:48 PM
20	Brighton Park, Lowe's Grocery, Wendys, library, Mint Hill Grill, mail box, Providence Produce	Jul 20, 2010 6:48 PM
21	I really can't get anywhere on foot from where I live without walking along dangerous roadways (Thompson Road and Idlewild Road)	Jul 21, 2010 8:16 PM
22	Dunkin Donuts	Jul 23, 2010 6:49 PM
23	Library, grocery store	Jul 23, 2010 8:07 PM
24	If we really wanted to...Lowes.	Jul 23, 2010 8:59 PM
25	nowhere There is not a sidewalk on the section of Lawyers road that I would have to walk to get to downtown Mint Hill. So, I don't feel like I can leave my neighborhood to walk to downtown Mint Hill.	Jul 27, 2010 3:15 PM
26	Town hall, carolina creamery, Dunkin Donuts	Jul 27, 2010 4:46 PM
27	Mint Hill Park on Fairview	Jul 29, 2010 1:44 PM
28	N/A.... no sidewalks	Aug 3, 2010 3:09 PM
29	downtown to parks	Aug 4, 2010 8:55 PM
30	Lowes, library, parks, restaurants, bank, Monroe Hardware and Town Hall but I can not all of these with connecting sidewalks and good crossing sites	Aug 11, 2010 4:47 PM
31	There are no sidewalks that lead anywhere from our neighborhood but I would like our family to be able to walk to Bain elementary & to the farmers market.	Aug 11, 2010 10:26 PM
32	Public Library, Dunkin Donuts, Baskin Robbins, new hardware store, Hawthornes, all places at Brighton Park.	Aug 12, 2010 9:43 PM
33	grocery, library, restaurants, doctor, bank, town hall	Aug 12, 2010 10:37 PM
34	Around Brighton Park	Aug 12, 2010 10:40 PM
35	Lowes Grocery Store, stores/restaurants in Lowes Plaza, MH Library	Aug 12, 2010 10:56 PM
36	We live in Brighton Park. We would patronize the businesses if they didn't keep going under. For instance we loved Mama's rest.	Aug 12, 2010 11:35 PM
37	Brighton Park shops	Aug 13, 2010 12:23 AM
38	Grocery store, restaurants in Brighton Park,	Aug 13, 2010 12:48 AM
39	Mint Hill Pavilion	Aug 13, 2010 12:54 AM
40	Town hall, library, grocery store, restaurants	Aug 13, 2010 12:59 AM
41	bus stop,police dept.,convenience store,food lion,dollar general,hawthorne's	Aug 13, 2010 1:27 AM
42	Lowes,	Aug 13, 2010 1:35 AM
43	Grocery store, Carolina Creamery, Hawthorne's, New Asian Cuisine, Dunkin' Donuts, Library	Aug 13, 2010 2:02 AM
44	Lowe's grocery	Aug 13, 2010 2:59 AM
45	Lowes Food, Jimmies, ABC Store, Mexican & Chinese Restaurants, Wendys	Aug 13, 2010 3:02 AM

## 6. Name some of the best places you can get to now on foot in Mint Hill.

	Response Text	
46	town hall, grocery store, restuarants, library	Aug 13, 2010 3:20 AM
47	Downtown	Aug 13, 2010 11:58 AM
48	Named above - all of Brighton Park, town center, etc. except I can't get there now without a sidewalk.	Aug 13, 2010 12:20 PM
49	Library, Coffee shop, Mint Hill deli, Lowes supermarket, Monroe Hardware and Hawthornes Restaurant. However, we are dissappointed that the downtown shopping center (near Lowes Supermarket) has so many empty storefronts.	Aug 13, 2010 3:18 PM
50	Brighton Park stores, Library, banking and Town Hall.	Aug 13, 2010 3:26 PM
51	businesses west of 51 and south of Lawyers, in the Brighton Park sector	Aug 13, 2010 4:14 PM
52	Library	Aug 13, 2010 4:36 PM
53	Grocery Store, Restaurants	Aug 13, 2010 5:16 PM
54	Lowe's supermarket Dunkin DoNuts Wendy's Would love a Dilworth Coffee House	Aug 13, 2010 8:42 PM
55	Libray Lowe's Dunkin Doughnuts	Aug 14, 2010 1:45 AM
56	Lowe's, Jimmies, Wendy's, and various other shops in the center	Aug 14, 2010 11:43 PM
57	lowes foods	Aug 15, 2010 2:41 AM
58	I walk everywhere downtown	Aug 16, 2010 12:31 AM
59	None - can not get there safely	Aug 16, 2010 7:37 PM
60	I cannot name any. I have to ride my bicycle to go into town center. walking aling HWY 51 is not easy.	Aug 17, 2010 12:49 AM
61	mint hill shopping area is good need crosswalk to library no sidewalks on that sidemore st lights	Aug 17, 2010 6:14 PM
62	our neighborgood need sidewalk on Lawyers Rd from Wilson Grove Road to NC 51 and bike lanes at least sidewalk	Aug 18, 2010 2:34 PM
63	Must walk along grass to get to library, grocery store, etc along Lawyers Rd	Aug 19, 2010 2:21 PM
64	It's only safe to walk within our neighborhood right now.	Aug 20, 2010 12:48 PM
65	Lowe's Food just because I'm in Brighton Park, but even inside the neighborhood sidewalks are incomplete. Getting to Hawthorne's is pretty easy but sidewalks are not available the entire trip. The most frustrating thing is the incompleteness of sidewalks on Hwy 51 and the fact that the sidewalk is only on one side of the street.	Aug 20, 2010 1:05 PM
66	Mint Lake Village is not linked to anything.	Aug 20, 2010 1:28 PM
67	from our neighborhood, we cannot walk anywhere because Margaret Wallace is a very busy road	Aug 21, 2010 8:23 PM
68	Just nearby neighborhood and side walks right in front of subdivision. Too much tall grass and no side walks in either direction with too many snakes, insects and other dead critters along the way. No side walk therefore must get in grass with the critters when car approaches.	Aug 23, 2010 9:52 PM
69	My neighborhood only.... please add something on Hwy 218 near our Neighborhood	Aug 25, 2010 7:04 PM
70	There are no sidewalks from our house to any place in Mint Hill. We have to walk in the grass to get to a sidewalk.	Aug 26, 2010 2:15 PM
71	Dilworth Coffee	Aug 26, 2010 7:26 PM
72	None	Aug 26, 2010 7:42 PM



## 6. Name some of the best places you can get to now on foot in Mint Hill.

	Response Text	
73	Nowhere from our neighborhood	Aug 26, 2010 7:47 PM
74	Brighton Park	Aug 26, 2010 7:53 PM
75	Around my neighborhood.	Aug 26, 2010 8:21 PM
76	None	Aug 26, 2010 8:21 PM
77	Oxfordshire only,so far.	Aug 27, 2010 1:39 AM
78	Mailbox	Aug 27, 2010 6:17 AM
79	nowhere - this town stinks for sidewalks in our area. we have been trying to get a 1/4 mile of sidewalk built for 5 years now.	Aug 27, 2010 11:52 AM
80	From our neighborhood the easiest place to get to on foot is the park on Wilgrove. Once you get there it is now easy to get into town.	Aug 27, 2010 12:03 PM
81	shopping centers, bank, town hall, restaurants...keep going with in mind!!!	Aug 27, 2010 12:27 PM
82	Brighton Park Lowes, library, Daphne's bakery, Mint Hill Grill, Subway, Fairview Park	Aug 27, 2010 1:05 PM
83	my neighbors house	Aug 29, 2010 2:13 AM
84	Lowe's Foods is the only destination because there are no sidewalks on Lawyers Road.	Aug 29, 2010 1:05 PM
85	Park on Wilgrove	Aug 30, 2010 2:39 AM
86	Parks	Aug 30, 2010 1:43 PM
87	cvs all my neighbors	Aug 30, 2010 8:26 PM
88	CVS Pharmacy, Food Lion, Mint Hill Parks (both)	Aug 31, 2010 8:49 PM
89	ABC Store and Library	Sep 1, 2010 1:24 PM
90	Grocery Store, Neighborhood, Library	Sep 1, 2010 1:57 PM
91	None - the sidewalks in our neighborhood do not connect to anything, including the adjoining neighborhood.	Sep 1, 2010 3:13 PM
92	None...there are no sidewallds leading from our neighborhood to the downtown area	Sep 2, 2010 12:46 AM
93	Brighton Park and sourounding businesses	Sep 6, 2010 9:12 PM
94	You can't get anywhere because none of the existing sidewalks connect to each other. My son walks to Independence High and is not consistently able to be on a sidewalk. Installing sidewalks at least a mile out from all of our schools should be a priority.	Sep 10, 2010 12:58 PM
95	Penny's Place, Showmar's, Jimmies, Historical Society/ farmer's market.	Sep 13, 2010 12:35 PM
96	none	Sep 23, 2010 5:48 PM
97	None from my house, need sidewalks.	Nov 4, 2010 4:41 PM
98	we only walk around the neighborhood as the sidewalks do not connect to any area outside of it. To go beyond our neighborhood, we drive.	Nov 19, 2010 1:36 AM
99	Brighton Park, Lowes Grocery, Ice Cream, Library, Hawthomes,	Nov 19, 2010 1:49 PM
100	My neighborhood and surrounding woods	Dec 1, 2010 12:30 AM
101	Ball park on Brief Rd., but we have to cut through a yard to do it.	Dec 2, 2010 12:37 AM
102	Nowhere! We are stuck in our neighborhood.	Dec 5, 2010 4:12 PM

## 7. Do you know of any areas where people have worn paths or formed

Response Text		
1	None	Jul 14, 2010 2:02 PM
2	Along Lawyers Road, during various times of the day you can see people struggling to transport themselves, groceries and children through the dirt and grass.	Jul 15, 2010 6:01 PM
3	No	Jul 16, 2010 12:27 PM
4	Lawyers Rd. from Hwy 51 to Truelight Church Rd.	Jul 16, 2010 9:09 PM
5	No	Jul 17, 2010 1:37 PM
6	Along side Lawyers Rd there is a worn path	Jul 17, 2010 2:15 PM
7	no	Jul 17, 2010 2:29 PM
8	Downtown Matthews, Miami, FL	Jul 17, 2010 5:57 PM
9	Yes! Need sidewalk to continue from Wendy's to the Dunkin' Donuts!	Jul 17, 2010 7:13 PM
10	worn Path from Wendy's to Duncan Donuts, worn path to Library from Mint Hill Family Practice	Jul 17, 2010 11:42 PM
11	no	Jul 18, 2010 3:25 PM
12	Entrance to the present Town Hall	Jul 18, 2010 5:35 PM
13	Lebanon Rd/Margaret Wallace Rd., SW from Lebanon Rd. Elementary School Lawyers Rd. NW from Lebanon Rd./Wilson Grove Rd. to post office and beyond	Jul 19, 2010 4:53 PM
14	no	Jul 19, 2010 7:48 PM
15	between Wendy's & Baskin Robbins on Hwy 51	Jul 20, 2010 6:48 PM
16	n/a	Jul 23, 2010 6:49 PM
17	Near Independence High School	Jul 23, 2010 8:07 PM
18	Don't know.	Jul 23, 2010 8:59 PM
19	no	Jul 27, 2010 3:15 PM
20	No.	Jul 27, 2010 4:46 PM
21	Lawyers Rd	Aug 3, 2010 3:09 PM
22	along Highway 51 between the Square and Wendy's	Aug 4, 2010 8:55 PM
23	Wendy's to the intersection of Lawyers. I have seen people walking along Lawyers near the town center. In Brighton Park there are unconnected sidewalks and the intersection from Evans Road to town.	Aug 11, 2010 4:47 PM
24	Definitely along Lawyers toward Lebanon rd. There are bus stops but no side walks.	Aug 11, 2010 10:26 PM
25	Yes! When coming from Brighton Park the sidewalk ends at Wendy's. Adding a sidewalk from there up to Lawyers would be great!!	Aug 12, 2010 9:43 PM
26	It would be nice to be able to cut up behind the Dunkin Donuts/Providence Market/Mr. Inspection to get to Mint Hill Deli and the events at Town Hall.	Aug 12, 2010 10:37 PM
27	Hwy. 51 closer around Idlewild has some gaps in sidewalk coverage  Lawyers from Evans Road to Lebanon Road	Aug 13, 2010 12:23 AM
28	From Brighton park to the intersection of Lawyers and Matthews-mint hill rd.	Aug 13, 2010 12:54 AM
29	connect side walk on 51 from Wendy's to Lawyer's Rd	Aug 13, 2010 12:59 AM
30	no	Aug 13, 2010 1:27 AM
31	All along Lawyers Rd towards CLT.	Aug 13, 2010 1:35 AM
32	From Lowe's Foods to Hawthorne's- Just past Wendy's, in front of Mr. Inspection, in front of the little salon and houses near town hall, also In front of the Library and down Lawyer's road towards the back our our neighborhood (Evan's road)	Aug 13, 2010 2:02 AM
33	Continue sidewalk from Wendy's to the corner of Lawyer's Road	Aug 13, 2010 2:59 AM

## 7. Do you know of any areas where people have worn paths or formed

	Response Text	
34	Wendys to town hall needs a complete sidewalk	Aug 13, 2010 3:20 AM
35	North Side of 51 between Brighton Park and Hawthorn's	Aug 13, 2010 4:42 AM
36	Along Lawyer's Road heading into town.	Aug 13, 2010 12:20 PM
37	yes, behind Brighton park subdivision	Aug 13, 2010 3:18 PM
38	west side of Hwy 51 should have completed sidewalk from CVS and First Baptist Church to Town Hall. south side of Lawyers Rd. between Evans Rd. and Hwy 51 should have a walkway.	Aug 13, 2010 4:14 PM
39	Nope	Aug 14, 2010 11:43 PM
40	lawyers road near brighton park up to traffic light on mathews mint hill 51.	Aug 15, 2010 2:41 AM
41	-Anywhere on Lawyers Road!!! Attaching Farmwood East to downtown would be a major plus. -Finishing the sidewalk on 51 between Wendy's and Dunkin Donuts (that is just plain crazy/cheap that the town won't finishing that piece until it is built on!!!) -Finishing the sidewalk on 51 between the Town Hall and Hawthorne's Pizza.	Aug 16, 2010 12:31 AM
42	from Wendy's down to Lawyer's Rd. and up Matthews Mint Hill Rd to Hawthorne's Pizza.	Aug 16, 2010 4:16 PM
43	Not at the present	Aug 17, 2010 12:49 AM
44	yes walking trail need longer sidewalk at the end of trail you run in to a low tree that need to be trim or cut back	Aug 17, 2010 8:14 PM
45	no	Aug 18, 2010 2:34 PM
46	Lebanon Rd to Hwy 51 Lebanon Rd to Margaret Wallace	Aug 18, 2010 8:09 PM
47	Areas near the cemetery and Danbrooke Park and other areas that frequently have walking pedestrians.  Would like sidewalks to begin at least from Heathlake since the mall is projected to be right around the curve/corner and will prove quite dangerous to attempt to walk to it.	Aug 23, 2010 9:52 PM
48	N/A	Aug 25, 2010 7:04 PM
49	No.	Aug 26, 2010 7:26 PM
50	on Truelight Church Rd. between Oxfordshire and The Oaks.	Aug 26, 2010 7:47 PM
51	No	Aug 26, 2010 8:21 PM
52	- Dunkin Donuts to Wendy's - Queens Grant (that side of street) to MH Library - need sidewalks on both sides for safety! - Need path from Lowe's Foods to MH Library - Jimmy's Restaurant to rest of shopping center (Not sure if sidewalks are now there or not - have not walked it in a while)	Aug 27, 2010 12:59 AM
53	Truelight Church Rd between Crown Hill connections. When to be built?	Aug 27, 2010 1:39 AM
54	Lawyers Road between 51 & Lebanon.	Aug 27, 2010 4:18 AM
55	Yes - between the entrance to the oaks at oxfordshire and oxfordshire neighborhoods - we're supposed to have a sidewalk built but nothing appears to be happening.	Aug 27, 2010 11:52 AM
56	I'm not aware of any.	Aug 27, 2010 12:03 PM
57	no	Aug 29, 2010 2:13 AM

## 7. Do you know of any areas where people have worn paths or formed

Response Text		
58	Lawyers Road is a critical road for sidewalks because it is on the bus route. It is a major road through the heart of Mint Hill and there are no sidewalks to connect it from Lebanon Road to 51. With sidewalks, people would be encouraged to walk to Lebanon Road School or Walgreens or Bi-Lo. Most importantly, the safety and encouragement of people riding the bus. No sidewalks on Lawyers Road, makes no sense!	Aug 29, 2010 1:05 PM
59	Highway 218, Lawyers Rd., Highway 51, Idlewild Rd.	Aug 30, 2010 1:43 PM
60	Yes	Sep 1, 2010 1:24 PM
61	Evans Rd close to Tutor Time and Hwy 51 near Dunkin Donuts	Sep 1, 2010 1:57 PM
62	There are foot paths along both sides of Lawyers Road	Sep 2, 2010 12:46 AM
63	from rt 51 down rt 218 to the park near rt 485	Sep 6, 2010 9:12 PM
64	Connect the existing sidewalks on Wilgrove-Mint Hill Road.	Sep 10, 2010 12:58 PM
65	Yes, Bartlett Rd. Some portions already have sidewalks but there are dangerous stretches that do not. This is a high traffic area with drop offs / no shoulder in several places. People from our neighborhood have hurt themselves by falling or turning over their ankle during their walk down Bartlett Rd. As a regular walker, I find that most drivers are courteous to the folks walking. Unfortunately, not all drivers are understanding &/or don't understand the problems walkers face when forced off the road into high grass, uneven ground, dropoffs, etc. Bartlett Rd. also is a route to the Park which creates another concern... there are sometimes children trying to walk against the traffic to get to the park. This is a particular problem when there are events at the park.	Sep 13, 2010 12:35 PM
66	Wilson Grove rd on way to independence Highschool	Sep 23, 2010 5:48 PM
67	Lawyers Road needs sidewalks bad.	Nov 4, 2010 4:41 PM
68	I see people walking up and down Mintwood between our neighborhood and Hwy 51.	Nov 19, 2010 1:36 AM
69	no	Dec 1, 2010 12:30 AM
70	A pathway to the ball park that would be paved so kids could ride their bikes and others could walk to watch the games would be great. Come into Ashe Plantation, turn left on Hanging Moss Trail. Turn left on Silver Maple. The path now exists between two homes.	Dec 2, 2010 12:37 AM
71	No.	Dec 5, 2010 4:12 PM

## 8. Are there any particular pedestrian improvements that you would like to see

Response Text		
1	no	Jul 14, 2010 2:02 PM
2	I would volunteer my time/materials to clear land for any greenway if possible.	Jul 15, 2010 3:49 PM
3	More sidewalks onlong Lawyers Rd. and finish sidewalks up Lawyers Rd.	Jul 15, 2010 5:02 PM
4	Widen Lawyers Rd Create an alternative way in and out than Albemarle and Lawyers rd route Add a couple stop signs to allow residents out of there communities into the flow of traffic Add a few lights on Lawyers Rd route More signage of areas of interest in the town More bike lanes & bike parking	Jul 15, 2010 6:01 PM



## 8. Are there any particular pedestrian improvements that you would like to see

Response Text		
5	Greenways are a tremendous improvement for other towns and would benefit Mint Hill a lot.	Jul 16, 2010 12:27 PM
6	sidewalks along every major street	Jul 16, 2010 4:48 PM
7	We would definitely walk the length of Lawyers Rd. from Hwy. 51 to as far as the Post Office. We would especially like a safe connection from our neighborhood to the Historical society and Mint Hill Arts.	Jul 16, 2010 9:09 PM
8	Greenways would be very nice. Also, sidewalks and well-lit walking areas would be nice.	Jul 17, 2010 1:37 PM
9	Paved sidewalks along Lawyers Rd toward I-485	Jul 17, 2010 2:15 PM
10	Enlarge the trail in the Park on Fairview would be nice. More lights(preferably amber) on route 51 and/or other roads where sidewalks could/would be added for safety purposes.W	Jul 17, 2010 2:29 PM
11	Need sidewalks on Lebanon Road to easy access to store. Is dangerous to ride bicycle on Lebanon Road as there is no much room to ride a bike with cars travelling.	Jul 17, 2010 4:21 PM
12	sidewalks, bicycle lanes, street lights	Jul 17, 2010 5:57 PM
13	more sidewalks along 51 from Lawyers to 218	Jul 17, 2010 11:42 PM
14	*Lawyers Road *Fill in gaps on 51 (between Wendys and Lawyers intersection) *Complete sidewalks on both sides of 51. *Improve intersection (like Lawyers/51) at 218 & 51. *Bike lanes to 51 and 218. *Plant street trees along 51 where possible,especially in front of Brighton Park.	Jul 18, 2010 3:25 PM
15	I think that reducing the speed limit to 35mph on Matthews-Mint Hill Rd from the Queen Grant Charter School to Lawyers Rd would aid pedestrians crossing the road without the undo burden on traffic that a "traffic control signal" would be.	Jul 18, 2010 5:35 PM
16	Would like to see sidewalk along Truelight Church Rd, from Oxfordshire Oaks subdivision to Wilgrove-Mint Hill Rd, and continuing from there to (1) Morgan Run subdivision (along Truelight ChurchRd) and to (2) Mint Hill Park along Wilgrove-Mint Hill Rd (where the new sidewalk currently ends).	Jul 18, 2010 11:26 PM
17	Sidewalk on NC-51 from Lebanon Rd. to Hoods Crossroads Sidewalk on NC-51 from Lebanon Rd. to Carving Tree Dr. Sidewalk on Lebanon Rd. from NC-51 to Lebanon Rd. Elementary School	Jul 19, 2010 4:53 PM
18	sidewalk between Wendy's & Baskin Robbins a crosswalk to Mint Hill Library sidewalk bet. Brighton Park and Lowe's Grocery (there is no safe place to walk through the parking lot between the ABC store and Earps to Lowe's)	Jul 20, 2010 6:48 PM
19	I would really like to see sidewalks/landscape on Thompson Road and on Idlewild between Rt 51 and I 485	Jul 21, 2010 8:16 PM
20	n/a	Jul 23, 2010 6:49 PM
21	All of Downtown (sidewalks or some form of pedestrian route on both sides of Hwy 51 and Lawyers throughout downtown)  All schools, parks and churches where there is potential or existing residential density to possibly support walking  Hoods Crossroads Shopping Center	Jul 23, 2010 8:07 PM
22	Sidewalks	Jul 27, 2010 3:15 PM
23	More sidewalks. I would love to be able to walk from our neighborhood to town hall.	Jul 27, 2010 4:46 PM

## 8. Are there any particular pedestrian improvements that you would like to see

	Response Text	
24	We'd love to see and use sidewalks up and down Lawyers Rd on both sides of Hwy 51	Aug 3, 2010 3:09 PM
25	connecting existing sidewalks where an area is missing	Aug 4, 2010 8:55 PM
26	I would like to see sidewalks completed along Rt 51 from at Phyliss Lane to Milgrove and Rt 218 The advertising for Brighton Park said we would be able to walk to 5 Parks. I can get to 1 using sidewalks. But entails crossing a major 2 lane where there are no sidewalks. I feel the businesses would be better served if there were sidewalks to their doors. If a serious commitment were made by our Mayor and Commissioners, the town could create a true "get around town center" atmosphere and effort to support businesses, visitors, workers, support people and residents.	Aug 11, 2010 4:47 PM
27	More sidewalks around Bain elementary.	Aug 11, 2010 10:26 PM
28	We have a hard time crossing from the shopping center over to the library - there need to be more sidewalks along the library side of the street and a way to get across - this is just one of the obvious areas we need to work on if we're to be a pedestrian-friendly town.	Aug 12, 2010 10:37 PM
29	Connect the sidewalk from Wendy's to intersection of 51 and Lawyers Rd.; need sidewalk from the new buildings on 51 (Orthodontist's building to Lawyers Rd to include Mint Hill Dentsry, MH Family Practice, Library to intersection)	Aug 12, 2010 10:56 PM
30	sidewalk on lawyers rd from 51 to lebanon rd.	Aug 12, 2010 11:35 PM
31	From Brighton park to the intersection of Lawyers and Matthews-mint hill rd.	Aug 13, 2010 12:54 AM
32	greenway	Aug 13, 2010 12:59 AM
33	a sidewalk from our development on bain school road(meadows of mint hill) to bain school. a sidewalk from our development on bain school rd to rte 218.	Aug 13, 2010 1:27 AM
34	When are the Greenways being built?	Aug 13, 2010 1:35 AM
35	More businesses to walk to! Crosswalks to get to Library and Showmar's shopping center	Aug 13, 2010 2:02 AM
36	I'd like to have sidewalk continued in front of Wendy's all the way to Lawyer's Road.	Aug 13, 2010 2:59 AM
37	There should be a sidewalk that connect the sidewalk that ends in front of Wendys with the town center.	Aug 13, 2010 3:02 AM
38	need crosswalk to library	Aug 13, 2010 3:20 AM
39	Specifically to Brighton Park, I hope the town is going to eventually (once all construction is completed) install speed bump. Cars just literally 'fly by'. I live at the intersection of Evans and Hove Rd and cars make that turn and come down the top of the hill at relatively high speed for being in a residential neighborhood. Also, there are no crossing paths for walkers anywhere around Brighton Park...yet.  Thank you for wanting to improve the wonderful town of Mint Hill! We love it!! <3	Aug 13, 2010 11:58 AM
40	Being able to cross street in mid-block (e.g., near library/downtown shopping center)	Aug 13, 2010 3:18 PM
41	I live at the corner of Evans Rd and Patriots Hill Rd and I would love to see a pedestrian water fountain that would also serve pets. While out in the yard I've had runners stop and ask if they could use my water faucet for themselves and also for their pets.	Aug 13, 2010 3:26 PM
42	All of those mentioned above. Plus, an active voice given to pedestrians and bike riders in budget planning and proactive visioning for the town's future.	Aug 13, 2010 4:14 PM
43	Sidewalks along Lawyers Road	Aug 13, 2010 4:36 PM



## 8. Are there any particular pedestrian improvements that you would like to see

	Response Text	
44	Extend the sidewalks at light to shopping at Hawthorne's Restuarant. The sidewalk ends before reaching the town center. Also it ends just past Wendy's and needs to be extended from there to the stop light.	Aug 13, 2010 5:16 PM
45	Would like to raise the issue of 4 way stop or at least a "School Bus Stop" sign at Eagle's Nest and Phyllis Lane which is the bus stop for Bain and Middle School children. People going and coming to back neighborhood via Phyllis Lane are continuously speeding and someone is going to get hurt.	Aug 14, 2010 11:43 PM
46	more sidewalks , some fountains as decor would be really pretty. the entrance to brighton park on evans road comming in from lawyers is to narrow. It is very hard for two cars to pass pulling in something has got to change there will be an accident.	Aug 15, 2010 2:41 AM
47	More sidewalks!  If you try to walk from Soaring Eagle Lane to Lowes Foods you end up walking through the busy parking lot. There is no pedestrian path in that retail center. You can walk all around it, but not straight through it.	Aug 16, 2010 12:31 AM
48	Absolutely 1. Provide a sidewalk along the length of Thompson Road from Lawyers to Idlewild. 2. Widen the road just a tad to allo for bicycle traffic. 3.Build sidewalks on Idlewild Road from near 485 to Margret Wallace Road	Aug 17, 2010 12:49 AM
49	see other comments above are below	Aug 17, 2010 6:14 PM
50	need sidewalk on Lawyers Rd from Wilson Grove Road to NC 51	Aug 18, 2010 2:34 PM
51	Sidewalks/greenway	Aug 18, 2010 6:09 PM
52	Sidewalks and bike lanes along Lawyers and 51	Aug 19, 2010 2:21 PM
53	From Mint Lake Village -- crossing Hwy. 51 to Hoods Crossroads shopping center -- very dangerous right now -- needs improvements.	Aug 20, 2010 12:48 PM
54	I feel in order for Mint Hill to become more walkable, it must first install complete sidewalks for the community to use. Having wider sidewalks, benches, more lighting, more cross walks, etc. are useless if there isn't a complete and connected sidewalk system. I fully support the existing future plan of having mixed retail/residential in downtown Mint Hill. In fact, that was one of the major points that encouraged me to purchase in Brighton Park. In my opinion, the future plan of drawing the community to down town will not succeed without extensive expansion of the sidewalk system.	Aug 20, 2010 1:05 PM
55	A sidewalk stretching from Hoods Cross Roads to downtown Mint Hill would be fabulous! If the Town of Matthews can do it, so can we.	Aug 20, 2010 1:28 PM
56	more sidewalks	Aug 21, 2010 8:23 PM
57	Biking trail and section portion of street like downtown has.  More side walks (Stevens Mills has a nice and long one directly across from the subdivision). The one next to Brighton park /lawyers that curves is nice and adds some challenge.  Keep the grass mowed; too tall.	Aug 23, 2010 9:52 PM
58	Bike/Walk ways Walk-a-thons	Aug 25, 2010 7:04 PM
59	Connectivity or circumnavigation of neighborhoods, i.e., down Lawyers Road to Apple Creek.	Aug 26, 2010 2:15 PM
60	Greenways, possibly, depending on the costs to the town financially and otherwise. I am against spending town money on sidewalks.	Aug 26, 2010 7:02 PM

## 8. Are there any particular pedestrian improvements that you would like to see

Response Text		
61	Sidewalks downtown	Aug 26, 2010 7:26 PM
62	See #5 above	Aug 26, 2010 8:18 PM
63	Sidewalks from: - Bain Elementary to Mint Hill Park on Fairview - Dunkin Donuts to Wendy's - Queens Grant (that side of street) to MH Library - need sidewalks on both sides for safety! - Need path from Lowe's Foods to MH Library - Jimmy's Restaurant to rest of shopping center	Aug 27, 2010 12:59 AM
64	I would love to see the sidewalk on Lawyers Rd. extended just past the Masons Lodge. That would open access to three neighborhoods directly into town.	Aug 27, 2010 1:18 AM
65	See #7 above.	Aug 27, 2010 1:39 AM
66	Yes - between the entrance to the oaks at oxfordshire and oxfordshire neighborhoods - we're supposed to have a sidewalk built but nothing appears to be happening.	Aug 27, 2010 11:52 AM
67	A complete, wide sidewalk connecting Bain to 51. Expansion of the sidewalk near Mint Hill Middle. Any type of sidewalk/path along the length of HWY 51	Aug 27, 2010 12:03 PM
68	More tree lined streets/sidewalks..	Aug 27, 2010 12:27 PM
69	Better sidewalk between Lawyers & 218 on 51 - very narrow with dangerous "bump-outs" at crosswalks directing walker onto 51 from sidewalk. Also, need to continue sidewalk down 51 between Brighton Park Wendy's and Lawyers. Lastly, need a crosswalk to library from Brighton Park.	Aug 27, 2010 1:05 PM
70	keep the sidewalks in the down town area. Spend the money on road improvements. Make Mint Hill the best town in Mecklenburg Co. with the best road system.	Aug 29, 2010 2:13 AM
71	Sidewalks, sidewalks, sidewalks everywhere!!! If you build it, they will come, people will get healthy, people will know their neighbors.	Aug 29, 2010 1:05 PM
72	more siadwalks with clear markings for pedestriains to cross would be nice !	Aug 30, 2010 2:39 AM
73	More sidewalks	Aug 30, 2010 1:43 PM
74	crossing refuge from mint lake village to dollar tree etc. would need a landing space on merchants side. hope they would be willing to provide that.	Aug 30, 2010 8:26 PM
75	All areas downtown	Sep 1, 2010 1:24 PM
76	Easier to cross Hwy 51 to Library	Sep 1, 2010 1:57 PM
77	The area from downtown on Lawyers Rd. to Thompson Rd.	Sep 1, 2010 3:13 PM
78	no	Sep 2, 2010 12:46 AM
79	more locations to cross rt 51 (between Lawyers and rt 218). More places for people to gather, ie fountains and seating area's near buisnesses. similar to those of Ballantyne area but with a look that compliments the structure in Mint Hill.	Sep 6, 2010 9:12 PM
80	Around all of our schools, especially Independence High.	Sep 10, 2010 12:58 PM
81	There is a shortage of streetlights along 218 & Bartlett Rd.	Sep 13, 2010 12:35 PM
82	Beter sidewalks and crossings near schools	Sep 23, 2010 5:48 PM
83	crosswalks, sidewalks.	Nov 4, 2010 4:41 PM
84	Start small. Connect the existing sidewalks before attempting sidewalks on both sides of street. One sided, continuous sidewalks would be a major leap forward from where we are today. If there is a need, and only if there is a need, the other side of the street could be added in the future.	Nov 19, 2010 1:36 AM

## 8. Are there any particular pedestrian improvements that you would like to see

Response Text		
85	Sidewalks down through rt 218 to get to the park near rt 485, currently too dangerous to do by bike or walk, have tried a few times.  A while back town had discussed an entry to a greenway at the corner of Lawyers and rt 51, if still on the table, that would be great, I believe yo would get more families out biking through neighborhood accessible green ways.	Nov 19, 2010 1:49 PM
86	no	Dec 1, 2010 12:30 AM
87	YES! A sidewalk that connects Ashe Plantation, Olde Sycamore, and Plantation Falls to the Fairview Rd. Park. Many families would use it.	Dec 2, 2010 12:37 AM
88	Sidewalk connecting Farmwood East to 51	Dec 5, 2010 4:12 PM

## 9. How satisfied are you with our current pedestrian infrastructure?

Comment		
1	what infrastructure?	Jul 16, 2010 4:48 PM
2	Unless you live off of Matthews-Mint Hill Rd there is no safe pedestrian access to the town's business district	Jul 17, 2010 2:15 PM
3	Currently I only walk on Kemper Lane or in the Park on Fairview	Jul 17, 2010 2:29 PM
4	Need more sidewalks	Jul 17, 2010 4:21 PM
5	sidewalks are too random.	Jul 20, 2010 6:48 PM
6	I enjoy going for a run/walk and have a difficult time doing that from where I live.	Jul 21, 2010 8:16 PM
7	Glad to see the addition of newest sidewalks this year.	Jul 29, 2010 1:44 PM
8	Connectivity is missing	Aug 11, 2010 4:47 PM
9	would be better with more crosswalk markings.	Aug 13, 2010 3:26 PM
10	pedestrian opportunities minimal; potential to extend and connect existing walkways is great	Aug 13, 2010 4:14 PM
11	Lived here over 30 years and asked for sidewalks, bike lane from Farmwood along Lawyers to no avail	Aug 19, 2010 2:21 PM
12	To get anywhere from Brighton Park, you have to walk through the front yards on Hwy 51.	Aug 20, 2010 1:05 PM
13	there are no sidewalks on any major road except downtow	Aug 21, 2010 8:23 PM
14	More and more walking, biking and CATs pedestrians so Mint Hill needs to adapt to the change, welcome and support it's changing community needs.	Aug 23, 2010 9:52 PM
15	I think we dont encourage this enough	Aug 25, 2010 7:04 PM
16	Need connectivity!!!!!!!!!!!!	Aug 27, 2010 12:59 AM
17	I love what I see so far!	Aug 27, 2010 1:18 AM
18	See #7 above.	Aug 27, 2010 1:39 AM
19	i cannot walk to my sister who is less then 1 mile away via Lawyers Road	Aug 27, 2010 4:18 AM
20	Disconnected due to lack of sidewalks.	Aug 29, 2010 1:05 PM
21	Sidewalks are limited to Matthews - Mint Hill Road	Sep 2, 2010 12:46 AM
22	I can see that an big effort is being made and that is satisfying.	Sep 10, 2010 12:58 PM
23	See 7 & 8.	Sep 13, 2010 12:35 PM
24	It's good now, but can be improved.	Dec 2, 2010 12:37 AM



## 10. What do you see as the Town's greatest STRENGTH in terms of walkability?

Response Text		
1	Shopping	Jul 15, 2010 5:02 PM
2	Park	Jul 15, 2010 6:01 PM
3	Enhancement of our small town feel and promotes good health.	Jul 16, 2010 12:27 PM
4	We use the new bridge to Brighton Park several time a week and feel much safer on that walk since it was put it.	Jul 16, 2010 9:09 PM
5	We are still a "small town" so we're not too spread out, which helps in terms of walkability.	Jul 17, 2010 1:37 PM
6	The down town area is well paved & easy to get around while walking	Jul 17, 2010 2:15 PM
7	The nature trail at the Park on Fairview and the paved track there provide a good walk but could be elongated.  I would like to see an ANNUAL WALK IN MINT HILL, preferably for a good cause, making the circle from Lawyers downtown, up Wilgrove-Mint Hill road, left on Wilson Grove road and back to Lawyers road. Should be 10 to 12 miles approx???	Jul 17, 2010 2:29 PM
8	Is a beautiful town and not congested as other areas in nearby areas.	Jul 17, 2010 4:21 PM
9	Vegetation, Sidewalk on Truelight Church and Lawyers Road intersection	Jul 17, 2010 5:57 PM
10	Relatively good network of street. Better connected than many areas of charlotte	Jul 18, 2010 3:25 PM
11	When choosing where to live when moving to greater Charlotte, I choose Mint Hill in general and Brighton Park in particular because of the amenities, like the library and perhaps a new YMCA to be built across Matthews-Mint Hill Rd. I loved the ability to walk to Lowe's Foods and buy fresh groceries almost daily.	Jul 18, 2010 5:35 PM
12	Sidewalks near the more recent housing developments.	Jul 18, 2010 11:26 PM
13	Only the sidewalk from the library to Fairview Rd. to the Park on Fairview.	Jul 19, 2010 4:53 PM
14	limited at best	Jul 19, 2010 7:48 PM
15	The intersection at hwy 51 & Lawyers is very nicely maintained. Great Job! The sidewalk through Brighton Park on Hwy 51 is nice and wide, also is well lit. We need more sidewalks in the town area.	Jul 20, 2010 6:48 PM
16	It's not that large of an area so it could be connected well, giving it more of a "town" feel.	Jul 21, 2010 8:16 PM
17	The desire from the public for a walkable community	Jul 23, 2010 8:07 PM
18	Creeks and power lines that can be made into nice Greenways.	Jul 24, 2010 10:23 PM
19	I don't think the town is walkable at all.	Jul 27, 2010 3:15 PM
20	Business in central area are in close proximity.	Jul 27, 2010 4:46 PM
21	parks	Aug 4, 2010 8:55 PM
22	There are some long sidewalks from downtown to the Park on Rt 218 and Bains School. But there are pedestrian hazards along the way.	Aug 11, 2010 4:47 PM
23	I like the paths in the park like Fairview park & also by Lowes Foods	Aug 11, 2010 10:26 PM
24	Brighton Park	Aug 12, 2010 9:43 PM
25	The many restaurants and the new businesses (dental offices?) going up (in a bad economy!) on 51. Lots of potential for this area to be a very walkable place to live.	Aug 12, 2010 10:37 PM
26	friendliness of residents	Aug 12, 2010 10:40 PM
27	Downtown area. Parks.	Aug 12, 2010 11:35 PM
28	Here in Brighton Park, they have done a great job of providing sidewalks and places for people to walk. However, until the construction is completed in certain areas, there are still areas where the sidewalks have not been completed, but that will take place when the construction is complete.	Aug 13, 2010 12:48 AM

## 10. What do you see as the Town's greatest STRENGTH in terms of walkability?

	Response Text	
29	Close proximity to multiple businesses and greenway	Aug 13, 2010 12:54 AM
30	safe. close proximity to attractions	Aug 13, 2010 12:59 AM
31	drivers respect for pedestrians.	Aug 13, 2010 1:27 AM
32	Location of area businesses.	Aug 13, 2010 1:35 AM
33	There are some businesses to walk to, and a grocery store in walking distance (we normally walk to the store instead of drive)	Aug 13, 2010 2:02 AM
34	Nothing	Aug 13, 2010 3:02 AM
35	lots of shops accessible on foot	Aug 13, 2010 3:20 AM
36	Close proximity of different things	Aug 13, 2010 4:42 AM
37	It has small condensed downtown areas that have the potential for working well for pedestrian traffic.	Aug 13, 2010 3:18 PM
38	Small hometown compact town layout.	Aug 13, 2010 3:26 PM
39	its size and layout may be totally accessible by foot with proper development of walkways; also Mint Hill has a small-town feel and atmosphere with a friendly reputation; improving the town's walkability will enhance this even further	Aug 13, 2010 4:14 PM
40	Because we live in Brighton Park we have the convenience of walking. The library is at the top of my list.	Aug 13, 2010 8:42 PM
41	walkin around brighton park	Aug 15, 2010 2:41 AM
42	That downtown is getting more businesses and more of an identity. Brighton Park really helped to get more residents downtown.	Aug 16, 2010 12:31 AM
43	1. There is actually space to build the sidewalks without worrying about properties. 2. There are places to walk to - grocery stores etc	Aug 17, 2010 12:49 AM
44	The thirst of going "local" i.e. farmer's market histroical society nursery businesses	Aug 18, 2010 6:09 PM
45	-Makes for connectivity -Better fitness of Town citizens -Safe transportation for walkers and bikers	Aug 19, 2010 2:21 PM
46	So much is located within one mile - two schools, two parks, diverse retail, and dining establishments. We have many reasons for people to walk, just not the infrastructure to support them walking safely.	Aug 20, 2010 1:05 PM
47	N/A	Aug 20, 2010 1:28 PM
48	none	Aug 21, 2010 8:23 PM
49	Has Trees and green ways/nature which makes it very beautiful for walking. Nice and quiet.	Aug 23, 2010 9:52 PM
50	You're survey and planning! Please keep this a nice smaller town, and not another Ballentyne unless you REALLY make it the nice side of Ballentyne. Keep your focus on keeping this a small community of neighbors and neighborhoods.	Aug 25, 2010 7:04 PM
51	None	Aug 26, 2010 2:15 PM
52	Brighton Park neighborhood connecting to Pavilion shopping center; Lawyers Glen walkway	Aug 26, 2010 7:26 PM
53	Matthews Mint hill road	Aug 26, 2010 7:42 PM
54	Should be able to walk around downtown	Aug 26, 2010 7:53 PM
55	Sidewalks	Aug 26, 2010 8:21 PM
56	Many diverse businesses and public places to go	Aug 26, 2010 8:21 PM

## 10. What do you see as the Town's greatest STRENGTH in terms of walkability?

	Response Text	
57	Variety of stores is available that you want to walk to different places, but they are not all connected so it is not easy to do so.	Aug 27, 2010 12:59 AM
58	Many great neighborhoods.	Aug 27, 2010 1:39 AM
59	Beautiful scenery and landscaping	Aug 27, 2010 4:18 AM
60	NOTHING - this town appears to have done little to nothing to allow walking and cycling in this community - it completely caters to the car.	Aug 27, 2010 11:52 AM
61	I think the town has a vision of what it wants to strive for as being a walk friendly community....stay focused on that idea and it will happen.	Aug 27, 2010 12:27 PM
62	Great sidewalk along Brighton Park subdivision (along 51)	Aug 27, 2010 1:05 PM
63	It could stay down town or in park and greenway areas.	Aug 29, 2010 2:13 AM
64	If you can arrive safely at 51, you can walk. It's getting there is the challenge.	Aug 29, 2010 1:05 PM
65	Lack of traffic congestion	Aug 30, 2010 1:43 PM
66	Excellent side walks along Hwy 51 and Wilgrove Road	Aug 31, 2010 8:49 PM
67	Downtown seems walkable, but you have to drive to get there.	Sep 1, 2010 3:13 PM
68	Nice downtown area that has a number of stores/restaurants	Sep 2, 2010 12:46 AM
69	sidewalks, the growth of more businesses along Rt 51	Sep 6, 2010 9:12 PM
70	The fact that the town recognizes that there is a great lack in pedestrian convenience.	Sep 10, 2010 12:58 PM
71	Mint Hill is a beautiful, quite town with great atmosphere & views. Nature is there to observe as well. I imagine there aren't too many places left where you can walk through downtown in the early morning & hear roosters.	Sep 13, 2010 12:35 PM
72	Doing something	Sep 23, 2010 5:48 PM
73	Downtown area of Mint Hill at Lawyers and 51.	Nov 4, 2010 4:41 PM
74	We love the spirit of Mint Hill and it's neighborhoods. Everyone (for the most part) is friendly and considerate.	Nov 19, 2010 1:36 AM
75	An increased compression of businesses near the town center, Brighton Park with it's gardens, benches and great walking paths have been a tremendous motivator to getting my family and neighbors to walk and bike outside of the Ellington Farms neighborhood.  The current town plan for growth seems to address many of the issues that will improve walkability, we need major points of interest such as Shopping, gardens, etc to get to for motivation to get out.	Nov 19, 2010 1:49 PM
76	low level of development makes it nice and safe to walk in neighborhoods and surrounding woods	Dec 1, 2010 12:30 AM
77	no comment	Dec 2, 2010 12:37 AM
78	New construction (ie Brighton park) has great access.	Dec 5, 2010 4:12 PM

## 11. What do you see as the Town's greatest WEAKNESS in terms of walkability?

	Response Text	
1	Walkability is most viable along the NC 51 corridor within a mile of the intersection of Lawyers Road.	Jul 15, 2010 3:27 PM
2	That we can walk some places just not nearly enough	Jul 15, 2010 6:01 PM
3	weakness not strength... how much are you paying surveymonkey?	Jul 16, 2010 4:48 PM



## 11. What do you see as the Town's greatest WEAKNESS in terms of walkability?

	Response Text	
4	I'm assuming this question is meant to ask the town's greatest "weakness" and not strength since that would be a repeat of the above question. So, I feel the greatest weakness is that there are no pedestrian connections from neighborhoods to other establishments such as parks, stores, library, etc.	Jul 17, 2010 1:37 PM
5	Same question as #10. If you meant "weakness" then I'd say its the lack of sidewalks off Lawyers Rd coming into down town from North & South	Jul 17, 2010 2:15 PM
6	same question as number 10 above	Jul 17, 2010 2:29 PM
7	Vegetation, Sidewalk on Truelight Church and Lawyers Road intersection	Jul 17, 2010 5:57 PM
8	duplicate question	Jul 17, 2010 11:42 PM
9	Having so many quality merchants and amenities within "walking distance." I even walk to Manchester Auto when leaving my car for service.	Jul 18, 2010 5:35 PM
10	Same question as above...did you perhaps mean Town's greatest WEAKNESS in terms of walkability? If so, I would say lack of sidewalks along busy stretches of roads like Wilgrove-Mint Hill Rd beyond the park.	Jul 18, 2010 11:26 PM
11	I assume you mean weakness id question 10 is strength. The greatest weakness is the lack of sidewalks.	Jul 19, 2010 4:53 PM
12	The greatest weakness in terms of walkability is the predominate development pattern (i.e. single family large lot subdivisions, disconnected land uses, lack of destinations)	Jul 23, 2010 8:07 PM
13	We need more sidewalks, especially on Lawyers Road between Matthews Mint Hill and Wilson Woods.	Jul 23, 2010 8:59 PM
14	Few sidewalks and no greenways. Also Mint Hill borders a less than desirable area of Charlotte and more sidewalks could result in people walking into neighborhoods were the added traffic is not wanted.	Jul 24, 2010 10:23 PM
15	lack of sidewalks	Jul 27, 2010 3:15 PM
16	Lack of sidewalks, and sidewalks that do exist, don't really go anywhere (new sidewalks along 51 as an example)	Jul 27, 2010 4:46 PM
17	sidewalks are not linked together in areas	Aug 4, 2010 8:55 PM
18	I see developing sidewalks but it seems much of the process seems disconnected.	Aug 11, 2010 4:47 PM
19	Along Highway 51 between Brighton Park and Lawyers	Aug 12, 2010 9:43 PM
20	Not building sidewalks and not connecting the ones here already.	Aug 12, 2010 10:37 PM
21	lack of sidewalks	Aug 12, 2010 10:40 PM
22	gaps in existing sidewalks leaving no alternatives except walking in ditch or roadway; too few sidewalks in place along Lawyers Rd.	Aug 12, 2010 10:56 PM
23	About evey where else.	Aug 12, 2010 11:35 PM
24	Chopped up side walks (stopping and starting of sidewalks)	Aug 13, 2010 12:54 AM
25	lack of sidewalks & greenway	Aug 13, 2010 12:59 AM
26	some areas do not have sidewalks	Aug 13, 2010 1:27 AM
27	Need more sidewalks along Lawyers Rd.	Aug 13, 2010 1:35 AM
28	There are many places where the sidewalk just ends, the Brighton Park shopping center has many empty businesses, would love to see businesses in there that we could walk to	Aug 13, 2010 2:02 AM
29	Sidewalks lack connectivity in the downtown area.	Aug 13, 2010 3:02 AM
30	need complete sidewalks and greenway	Aug 13, 2010 3:20 AM
31	Number of sidewalks	Aug 13, 2010 12:20 PM
32	There is need for more sidewalk, trees, rest areas for sitting....	Aug 13, 2010 3:18 PM

## 11. What do you see as the Town's greatest WEAKNESS in terms of walkability?

	Response Text	
33	Need crosswalk marking to cross Hwy 51 to Library and more restaurant would be greatly appreciated in the downtown area or close/in Brighton Park.	Aug 13, 2010 3:26 PM
34	definite hazard zones, intersections, and crossings for the pedestrian or cyclist	Aug 13, 2010 4:14 PM
35	no side walks on lawyers to walk from brighton park	Aug 15, 2010 2:41 AM
36	The break in sidewalk coverage, or no sidewalks in some places...like Lawyers Road.	Aug 16, 2010 12:31 AM
37	Sidewalks do not connect. Too many long breaks.	Aug 16, 2010 4:16 PM
38	safety - when using other than auto transport; safety is the number one concern.	Aug 16, 2010 7:37 PM
39	1. Sidewalks on only one side of the roads. 2. Sidewalks that run for a few yards and end. 3. The newest neighborhood at the corner of Lebanon and 51 put sidewalk too close to 51 traffic. 4. Sidewalks that go nowhere!!	Aug 17, 2010 12:49 AM
40	sidewaks and handicap	Aug 17, 2010 6:14 PM
41	no sidewalk	Aug 18, 2010 2:34 PM
42	No connectivity between surrounding neighborhoods to businesses	Aug 18, 2010 6:09 PM
43	-Poor for outlying neighborhoods	Aug 19, 2010 2:21 PM
44	Too few areas to safely walk.	Aug 20, 2010 12:48 PM
45	Lack of complete sidewalk paths.	Aug 20, 2010 1:05 PM
46	Only those that live in or near downtown Mint Hill can experience the pedestrian infrastructure, without driving to walk.	Aug 20, 2010 1:28 PM
47	except for downtown you can only walk safely in your own neighborhood. Children who live less than a half mile from their school must ride school buses.	Aug 21, 2010 8:23 PM
48	Virtually no sidewalks.	Aug 23, 2010 9:52 PM
49	Not sure.. other than lack of trails or awareness of them. We have a larger residential area OUTSIDE of the main Downtown area, so how to you stimulate this for the outlying areas?	Aug 25, 2010 7:04 PM
50	Not enough sidewalks and sidewalks on just one side of the street. No connectivity or loops.	Aug 26, 2010 2:15 PM
51	No sidewalks downtown	Aug 26, 2010 7:26 PM
52	No sidewalks on Idlewild, Thompson or Lawyers to get to rt.51	Aug 26, 2010 7:42 PM
53	Should be able to walk around downtown	Aug 26, 2010 7:53 PM
54	Not many connected sidewalks	Aug 26, 2010 8:21 PM
55	Only looking out for certain areas. Ashe Plantation cannot get paving or sidewalks.	Aug 26, 2010 9:34 PM
56	No Connectivity!!!!!!!!!!!!!!	Aug 27, 2010 12:59 AM
57	Need more sidewalks.	Aug 27, 2010 1:39 AM
58	No sidewalks on busy roads, lighting very poor at night.	Aug 27, 2010 4:18 AM
59	Failure by the town to always require the installation of combination curb and gutter along with sidewalks when approving new developments and schools.	Aug 27, 2010 6:17 AM
60	EVERYTHING - this town appears to have done little to nothing to allow walking and cycling in this community - it completely caters to the car.	Aug 27, 2010 11:52 AM
61	Drivers need to be more aware of the pedestrians and allow the pedestrians (and bicylist) the right of way.	Aug 27, 2010 12:03 PM
62	Not enough TREE-LINED sidewalks	Aug 27, 2010 12:27 PM
63	Need greater access.	Aug 27, 2010 1:05 PM

## 11. What do you see as the Town's greatest WEAKNESS in terms of walkability?

Response Text		
64	too many cars	Aug 29, 2010 2:13 AM
65	I feel like a broken record with this survey. It's all about the lack of sidewalks. There's no connectivity. People are isolated on islands with no bridges to connect to other areas of the town. No bike paths, no greenway. With all the open spaces of land in Mint Hill, this just seems impossible to understand how we arrived at this point with no greenways or bike paths or SIDEWALKS!	Aug 29, 2010 1:05 PM
66	Lack of sidewalks, greenways, trails	Aug 30, 2010 1:43 PM
67	you can't get very far without running out of sidewalk	Aug 30, 2010 8:26 PM
68	Lawyers Rd area from Evans Road to Town Square	Aug 31, 2010 8:49 PM
69	Same as 10.	Sep 1, 2010 3:13 PM
70	No sidewalks on Lawyers Road leading into the town center of Mint Hill	Sep 2, 2010 12:46 AM
71	lack of interconnectedness between neighborhoods	Sep 6, 2010 9:12 PM
72	None of the existing sidewalks connect to each other.	Sep 10, 2010 12:58 PM
73	Lack of sidewalks & too few streetlights. Also, many sections of sidewalk along 218 are often overgrown to the point there is only a small single path down the middle of the sidewalk barely passable by 1 person at a time. As a point of reference, the sidewalk across from McWhirter Grading is often overgrown. It sometimes goes weeks without attention.	Sep 13, 2010 12:35 PM
74	Taking to long	Sep 23, 2010 5:48 PM
75	Lack of sidewalks connecting nearby neighborhoods to shopping centers.	Nov 4, 2010 4:41 PM
76	There isn't an old town center full of shops and restaurants with possibly residential above. Matthews, Davidson and Pineville have true "downtowns". This could be remedied by building up the area around the old and new town halls, connecting Lawyers and Nelson/Despa to Mint Hill Village and the new town hall.	Nov 19, 2010 1:36 AM
77	Lack of walkways and interconnecting neighborhoods, the distance between places to walk are a factor,	Nov 19, 2010 1:49 PM
78	none	Dec 1, 2010 12:30 AM
79	The Plan to disrupt and disturb natural wildlife....Pour sidewalks in current neighborhoods....leave nature alone.	Dec 1, 2010 7:46 PM
80	no comment	Dec 2, 2010 12:37 AM
81	Continuous connection	Dec 5, 2010 4:12 PM

## 12. Do you feel that the Town's pedestrian infrastructure promotes safety?

Other (please specify)		
1	Very unsafe, especially on Lawyers Road and forget crossing on Pineville-Matthews	Jul 15, 2010 6:01 PM
2	Not currently but with added greenways, it would.	Jul 16, 2010 12:27 PM
3	Not on the strip of Lawyers Rd. mentioned above.	Jul 16, 2010 9:09 PM
4	Only along Matthews - Mint Hill Rd	Jul 17, 2010 2:15 PM
5	The entrance to Mint Hill Deli, to name one, with cars turning left into their lot could be very dangerous to pedestrians and parking in the lot across from the Mint Hill deli should be allowed. Crosswalks downtown should have the signs that show the hands and give you 30 seconds to cross safely from one side to the other.	Jul 17, 2010 2:29 PM



## 12. Do you feel that the Town's pedestrian infrastructure promotes safety?

Other (please specify)		
6	It is getting there. Sidewalks just need connecting along the major thoroughfares.	Jul 18, 2010 5:35 PM
7	Only in so far as where there are sidewalks is the pedestrian infrastructure safe.	Jul 19, 2010 4:53 PM
8	I see many people running across Hwy 51 at the library and on Lawyers Rd. to the Mint Hill Grill	Jul 20, 2010 6:48 PM
9	I believe there needs to be a better pedestrian infrastructure taking more from a "rural" atmosphere to a "town" type atmosphere.	Jul 21, 2010 8:16 PM
10	There are exceptions but for the most part pedestrian infrastructure in Mint Hill is an afterthought and usually done just to meet minimum requirements	Jul 23, 2010 8:07 PM
11	Many places where people would walk do not have sidewalks.	Jul 27, 2010 4:46 PM
12	areas where people have to walk out in street to go from one area of sidewalk to another area	Aug 4, 2010 8:55 PM
13	With work, it can improve. When the sidewalks end and you are forced to cross the street safety becomes an issue. Pedestrian crosswalks and light will help at major intersections, Traffic continues to get heavier so safety issues become doubly important. Are you ready for bike's yet?	Aug 11, 2010 4:47 PM
14	I just dont think there are enough sidewalks to access so it is not safe to walk anywhere.	Aug 11, 2010 10:26 PM
15	Quite dangerous to cross 51.	Aug 12, 2010 10:37 PM
16	I think there are good intentions but poor planning.	Aug 12, 2010 11:35 PM
17	More continuous sidewalks would allow better safety, rather than walking in the grass, right along side busy roads	Aug 13, 2010 12:54 AM
18	It would be better if there were continuous sidewalks	Aug 13, 2010 2:02 AM
19	Could be safer in high traffic areas	Aug 13, 2010 12:20 PM
20	However, additional sidewalks are necessary to help with walkability.	Aug 13, 2010 3:18 PM
21	Hwy 51 carries a lot of traffic by Brighton Park and I understand it's a State road and we might not be able to control or add cross walk markings.	Aug 13, 2010 3:26 PM
22	the aforementioned needs for crossings and sidewalk completions have been needed for a long time (years) ; a preventive and proactive response should be forthcoming before injury or fatality could result	Aug 13, 2010 4:14 PM
23	need more sidewalks by wendys down to dunkin dounuts and turning on lawyers down to brighton park	Aug 15, 2010 2:41 AM
24	I feel the sidewalks have been completed by builders/developers and if the Town cared about safety at all, the piece between Wendy's and Dunkin Donuts on 51 would be connected as would the piece between Town Hall and Hathorne's. It is obviously all about money, not safety.	Aug 16, 2010 12:31 AM
25	I answered yes, but with a caveat. {ut sidewalk on both sides of the roads! And create a lane for at least one bicycle.	Aug 17, 2010 12:49 AM
26	somewhat at times it depends	Aug 17, 2010 6:14 PM
27	in terms of biking and walking to town along Lawyers Rd	Aug 19, 2010 2:21 PM
28	With some improvements.	Aug 20, 2010 12:48 PM
29	At this point the current infrastructure does not because it is incomplete. In addition to walking in people's yards on Hwy 51, having such a small sidewalk beside a 4 lane road without some distance from the road is not safe. In addition, the sidewalk that is present is narrow especially when other people share the sidewalk.	Aug 20, 2010 1:05 PM
30	Not sure	Aug 25, 2010 7:04 PM
31	For bicycle safety too	Aug 26, 2010 8:21 PM
32	Only in certain locations in the center of town	Aug 27, 2010 6:17 AM

## 12. Do you feel that the Town's pedestrian infrastructure promotes safety?

Other (please specify)		
33	I think the town is giving it a good try, but more progress needs to be made.	Aug 27, 2010 12:27 PM
34	There's no sidewalks through the heart of Mint Hill. You have a road with a 45 mile an hour speed limit, which people constantly exceed, and no place for people to walk, even to ride the bus. How is that safe!	Aug 29, 2010 1:05 PM
35	Too many sidewalk breaks and no sidewalks along major roads.	Sep 1, 2010 3:13 PM
36	based on what has been put into place during the last 5 years	Sep 6, 2010 9:12 PM
37	no opinion	Dec 1, 2010 12:30 AM
38	no.....sidewalks will be suffice to promote safety....that will not add to water pollution/drainage or wildlife disruption	Dec 1, 2010 7:46 PM

## 13. Any additional comments?

Response Text		
1	Thanks,	Jul 15, 2010 6:01 PM
2	We love Mint Hill and making it a pedestrian friendly town is a high priority for us.	Jul 16, 2010 9:09 PM
3	Putting sidewalks along Lawyers Rd would enable all of the residents safer & more convenient pedestrian access to the business district of down town which would decrease car traffic & parking issues as well as increase consumer shopping in the area.	Jul 17, 2010 2:15 PM
4	When I moved to Mint Hill in 1983 I walked within the Heathergate subdivision and I am sure most subdivisions have walkers but I believe the town needs to address more lights, other than the ones approved by the state, are needed for safety and protection at night. Having lights only on the corners is ok but the distances between the corners in some instances make for very dimly lit areas. We have had break ins on our little street over the years as I know others have had. WE NEED MORE LIGHTS IN MINT HILL for our safety as well as to give us the ability to walk in the cool pm hours.	Jul 17, 2010 2:29 PM
5	Truelight Church does not have any sidewalks so, I am forced to walk on people's lawns. There is a similar problem on the Lawyers Road and Mint Hill-Matthews Road intersection.	Jul 17, 2010 5:57 PM
6	A greenway would be great! That is the reason we moved to Brighton Park. We were told that there would be one soon.	Jul 17, 2010 7:13 PM
7	Downtown Mint Hill should be the focus. It is important to the welfare of the businesses and the citizens to create a highly walkable downtown.	Jul 18, 2010 3:25 PM
8	I love Mint Hill and appreciate all of the efforts of the many who have participated in making this such a great place to live.	Jul 18, 2010 5:35 PM
9	i do not see lowering the speed limit on Hwy 51 past Brighton Park as I believe this will cause traffic issues. The 35 mph could start at Phyllis Ln going in to town.	Jul 20, 2010 6:48 PM
10	I hope the Mayor, Commissioners, Town Manager commit and take a stance to fund and support the best of the Plan. Joe Simino Brighton Park	Aug 11, 2010 4:47 PM
11	Thanks for asking!!	Aug 12, 2010 9:43 PM
12	I see great possibilities for this town!	Aug 12, 2010 10:37 PM
13	I would like to see a sidewalk from the library down to the food lion on both sides of the road. I would like to feel safe walking from Brighton Park to the Sat Market, Food Lion, and the Ice cream shop in the center of town.	Aug 12, 2010 10:40 PM

### 13. Any additional comments?

	Response Text	
14	Brighton Park is a wonderful neighborhood for walking. We would like to see sidewalks that connect this neighborhood to the center of Mint Hill so that we could walk to banks, more shopping and restaurants without having to walk in unsafe areas.	Aug 12, 2010 10:56 PM
15	More needs to be done to promote a vibrant down town. The police need to be less aggressive.	Aug 12, 2010 11:35 PM
16	It would be nice to walk on sidewalks from Brighton Park to Hawthorn's restaurant. There's only a short part of this route that is missing sidewalk.	Aug 13, 2010 2:59 AM
17	I hope I live long enough to be able to walk safely all over Mint Hill	Aug 13, 2010 3:02 AM
18	We moved to Mint Hill (from South Charlotte) because the town seemed intent on "smart growth" (with a comprehensive land use plan and pedestrian friendly downtown area). Our hope is that "smart growth" will prevail.	Aug 13, 2010 3:18 PM
19	How about finishing the sidewalk in front of AT&T/ice cream store up to Wendy's. And please don't ever change the tree canopy coming into Mint Hill on Lawyers Rd. One of the things that drew me to Mint Hill was the welcoming shady path down Lawyers to town. I love trees!	Aug 13, 2010 3:26 PM
20	feel free to contact me. I walk daily throughout all areas of Mint Hill. I see constantly where improvements could be implemented. 704-545-0498 Stephen Uzzell	Aug 13, 2010 4:14 PM
21	We have a problem with people who allow their pets to pee on our grass in our yard. We have numerous spots in the grass both front, back and side where the animals have killed our grass. It is trespassing on our property. We unfortunately have sidewalks along the front and side of our home ( corner lot) which people use daily. There are too many pet owners in this neighborhood who do not respect another person's property.	Aug 13, 2010 5:16 PM
22	See #8 above	Aug 14, 2010 11:43 PM
23	Taking into consideration bikes would also be great!	Aug 16, 2010 12:31 AM
24	We bike ride as well as walk. There are no "safe" routes to get to downtown Mint Hill. How about a future project with the town of Matthews to connect the two? I believe this would promote commerce. People obviously are becoming more "green/eco-friendly" - let's jump on board! :)	Aug 18, 2010 6:09 PM
25	Need sidewalks, bike lanes before greenways	Aug 19, 2010 2:21 PM
26	I've only lived in Mint Hill since October 2009 but I really love it here. I'm excited about the expansion of down town and being able to take advantage of the fact that so much is available within walking distance (schools, parks, retail, dining, etc.). If the leadership of Mint Hill has the courage to take a chance on following through with the proposed future plan of Mint Hill it will quickly gain the positive reputation and respect it will deserve from the greater Charlotte-Mecklenburg community. With recognition and a positive reputation will come more people moving here along with their tax dollars. Do not minimize the strength of this community when it comes to banding together for the greater good of Mint Hill. Look at how the community stepped up to save the library.	Aug 20, 2010 1:05 PM
27	I vote "NO", on a new townhall.	Aug 20, 2010 1:28 PM
28	If sidewalks are build along Margaret Wallace and other major roads that connect to Margaret Wallace, I would be a frequent user for both errands and pleasure walking. I'm sure many people would use whatever sidewalks were built and it would be better for the health of both audlts and children.	Aug 21, 2010 8:23 PM

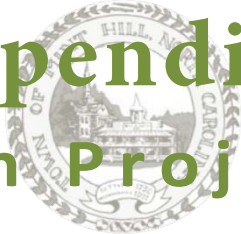


### 13. Any additional comments?

Response Text		
29	Walking, Biking, Skateboarding promotes health and exercise for families. Lack of sidewalks prevents much of this without getting in a car and traveling to an area that has side works, paved designed areas specific to these needs.  Would also be nice to have community shuttles that would take family/residents to some areas that may have sidewalks to walk etc or take them to CATs pickups since there are not side walks in many neighborhoods.	Aug 23, 2010 9:52 PM
30	Thanks for taking the time to make this a priority. Build them, light them and encourage us to use them.	Aug 25, 2010 7:04 PM
31	This focus on pedestrians. I am avid bike rider and do the narrow shoulders on most of our rides and very little pedestrian activity, it is safer to ride my bike on sidewalks. I am thrilled with the Wilgrove Mint Hill sidewalk and use it almost weekly to go to Bank of America, Foodlion, CVS, and Mint Hill Vet. Bike racks at these establishments would also be a benefit. CVS has told me they cannot accommodate a bike rack. One reason given was that a permit would be needed. Is this true? Also, funding. Anyway some of this plan could include bike racks at some major businesses. Walgreen's on Lawyers Rd has accommodated me.	Aug 26, 2010 8:21 PM
32	Connect the sidewalks on both sides of Hwy 51 from Hwy 218 to Queen's Grant/doctor's offices, at the minimum.	Aug 27, 2010 12:59 AM
33	More sidewalk for Truelight Church Rd.	Aug 27, 2010 1:39 AM
34	We have a long ways to go to promote walking and cycling in this community - this town has done virtually nothing to promote either - instead relying on narrow roads and cars only for getting anywhere	Aug 27, 2010 11:52 AM
35	Any improvements to the downtown area of Mint Hill is a plus for everyone who lives here. More sidewalks and MORE TREES will enhance the beauty of this town.	Aug 27, 2010 12:27 PM
36	not at this time	Aug 29, 2010 2:13 AM
37	I really believe it's a no brainer that sidewalks are desperately needed along Lawyers Road, which is a BUS ROUTE!!!! Why you used grant money to fund sidewalks on Wilgrove-Mint Hill which isn't even on the bus route, is beyond my comprehension. Just look at the path worn in the grass on the Lawyers Road Speedway and you'll see where the sidewalks are needed, Please!!!!	Aug 29, 2010 1:05 PM
38	Making Mint Hill more 'walkable' would help create a friendlier atmosphere as well as help encourage better health. It could be very helpful in a gas crisis.	Aug 30, 2010 2:39 AM
39	must make it possible for more people to walk. save on gas and get more exercise. and it is fun.	Aug 30, 2010 8:26 PM
40	We went to the August 31st meeting and were excited about the proposed greenway along Lawyers Rd. It would go across the corner of our lot. It would tremendously increase my wife's independence and mobility.	Sep 1, 2010 3:13 PM
41	Many of us would like to see a great plan put into place and executed, a town feels alive when it's residents are out and about on foot or bike...keep up the great work.	Sep 6, 2010 9:12 PM
42	I think it is a great idea to be looking into our town's sidewalk & streetlight issues. It shows the progressive, proactive leadership that makes this town a great place to live. To all that have taken on this task...Thank you. Keep up the great work.	Sep 13, 2010 12:35 PM
43	Rather than spending time on Grants why not just go do the sidewalks	Sep 23, 2010 5:48 PM
44	The fact that these discussion are going on with the intent to make improvements are a great benefit to the current and future residents of Mint Hill, It cannot come fast enough, Mint Hill can be a shining example of how to create a great livable community, my only complaint is that changes have come much too slow in implementing some of the great changes and plans that have been discussed and that are currently on the board.	Nov 19, 2010 1:49 PM

### 13. Any additional comments?

Response Text		
45	I would move if a greenway were to be built in my backyard. I will do everything possible to petition my neighbors to act against such a development. A greenway in my backyard would consume much of my private property, destroy my privacy, invite criminals with easy access to my property	Dec 1, 2010 12:30 AM
46	As you can read I am against the plan o disturb wildlife along Irvin Creek and will do my best to protest and gather up others to fight against this plan	Dec 1, 2010 7:46 PM
47	A sidewalk from Farmwood East would connect neighborhoods as far away as Cheval to downtown Mint Hill.	Dec 5, 2010 4:12 PM



# **Appendix B**

## **Pedestrian Project Matrix**



## MINT HILL PEDESTRIAN MASTER PLAN

Sidewalk Projects										
Project ID	Project Location	From	To	Side of Road	Length (mi)	Rating Score	Improvements Needed	Scheduled Priority	Funding Source	Responsibility
13	NC 51	Phylliss Ln	Mint Hill Family Practice	South side	0.14	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
14	NC 51	Hawthorne Dr	North of Evans Rd (Showmars)	Southeast side	0.25	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
15	NC 51	North of Evans Rd (Wendy's)	Dunkin' Donuts	Northwest side	0.05	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
16	NC 51	Southeast side of Dunkin' Donuts	Lawyers Road	Northwest side	0.04	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
17	NC 51	Warehouse Rd	West side of Hawthorne's Pizza	North side	0.07	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
18	NC 51	East side of Hawthorne's Pizza	Mint Hill Village Ln	North side	0.03	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
19	NC 51	East side of Mint Hill Village	Southwest of Sunset Dr	Northeast side	0.29	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
41	Lawyers Rd	Truelight Church Rd	North of NC 51	East side	0.58	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
42	Lawyers Rd	NC 51	North of Hollow Oak Dr	West side	0.46	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
43	Lawyers Rd	Nelson Rd	North of NC 51	West side	0.28	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
44	Lawyers Rd	South of NC 51	North side of BB&T	East side	0.02	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
46	Lawyers Rd	South side of BB&T	Shelbourne Pl	East side	0.63	70	5-ft sidewalk	Short-Term	L/S	MH/NCDOT
82	Bain School Rd	NC 51	Philadelphia Church Rd	North side	0.20	70	8-10 ft sidewalk	Short-Term	L/S	MH/NCDOT
58	Wilgrove Mint Hill Rd	Pine Hill Rd	South of Nelson Rd	West side	0.10	65	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
59	Wilgrove Mint Hill Rd	Pine Hill Rd	NC 51	East side	0.39	65	8-10 ft sidewalk	Mid-Term	L/S	MH/NCDOT
63	NC 218	NC 51	Jefferson Colony Rd	North side	0.76	65	8-10 ft sidewalk	Mid-Term	L/S	MH/NCDOT
11	NC 51	Zeb Morris Wy	Robin Hollow Dr	East side	1.35	61	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
12	NC 51	North side of Budget Self Storage	Carving Tree Dr	West side	1.02	61	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
39	Lawyers Rd	Lebanon Rd	Truelight Church Rd	South side	1.53	57	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
40	Lawyers Rd	Wilson Grove Rd	Truelight Church Rd	North side	1.52	57	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
20	NC 51	Woodland Ave	I485	East side	1.54	52	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
32	Lebanon Rd	Barnsdale Ln	North of Morris Park Dr	West side	1.48	52	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
33	Lebanon Rd	North of Gristmill Ln	North of Morris Park Dr	East side	1.31	52	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
60	Wilgrove Mint Hill Rd	South of Nelson Rd	NC 51	West side	0.17	52	8-10 ft sidewalk	Mid-Term	L/S	MH/NCDOT
61	NC 218	NC 51	Philadelphia Church Rd	South side	0.07	52	8-10 ft sidewalk	Mid-Term	L/S	MH/NCDOT
84	Bain School Rd	Philadelphia Church Rd	Gait Dr	East side	0.62	52	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
124	Philadelphia Church Rd	Bain School Rd	NC 218	East side	0.23	52	8-10 ft sidewalk	Mid-Term	L/S	MH/NCDOT
30	Lebanon Rd	Margaret Wallace Rd	Barnsdale Ln	East side	0.70	48	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
38	Wilson Grove Rd	Flaxton Dr	Wilgrove Mint Hill Rd	West side	1.25	48	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
45	Lawyers Rd	South of Hollow Oak Dr	Shelbourne Pl	West side	0.28	48	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
83	Bain School Rd	Philadelphia Church Rd	Gait Dr	West side	0.62	48	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
103	Truelight Church Rd	Lawyers Rd	Versage Dr	West side	0.54	48	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
105	Truelight Church Rd	North of Heather Nicole Ln	North of Chicopee Dr	East side	0.50	48	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
27	Margaret Wallace Rd	Stilwell Rd	Lebanon Rd	North side	0.29	44	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
28	Lebanon Rd	Margaret Wallace Rd	South of Lawyers Rd (Walgreens)	East side	0.34	44	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
29	Lebanon Rd	Trinity Medical Center	Margaret Wallace Rd	West side	0.14	44	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
51	Lawyers Rd	Northwestern Town limits	Wilson Grove Rd	North side	1.00	44	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
52	Lawyers Rd	Northwestern Town limits	South of US Post Office	South side	0.85	44	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
62	NC 218	Cagle Dr	Large Oak Ln	South side	0.04	44	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
3	Idlewild Rd	Mint Hill Middle School bus entrance	South of Morning Star Dr	East side	0.67	39	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
10	NC 51	North of David Dr	Zeb Morris Wy	West side	0.28	39	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
21	NC 51	Wilgrove Mint Hill Rd	I-485	West side	1.69	39	8-10 ft sidewalk	Mid-Term	L/S	MH/NCDOT

Agency/Department	Funding Sources	
MH - Mint Hill  MUMPO - Mecklenburg-Union Metropolitan  NCDOT - North Carolina Department of  CTT - Carolina Thread Trail	L - Local	N - National
	General Fund	Transportation Enhancement Program
	Road Improvement Funds	CMAQ
	Powell Bill Funds	P - Private
		Carolina Thread Trail
	S - State	Developers
	Safe Routes to School	
	Governor's Highway Safety Program	
	North Carolina Parks and Recreation Trust Fund	
	North Carolina Conservation Tax Credit Program	
	North Carolina Trails Program	

Sidewalk Projects										
Project ID	Project Location	From	To	Side of Road	Length (mi)	Rating Score	Improvements Needed	Scheduled Priority	Funding Source	Responsibility
23	NC 51	I-485	South of Jomac Dr	East side	1.54	39	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
34	Lebanon Rd	Morris Park Dr	NC 51	East side	0.15	39	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
35	Lebanon Rd	South of Morris Park Dr	NC 51	West side	0.14	39	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
111	Truelight Church Rd	North of Crown Hill Dr	NC 51	North side	0.83	39	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
123	Philadelphia Church Rd	Bain School Rd	NC 218	West side	0.23	39	8-10 ft sidewalk	Mid-Term	L/S	MH/NCDOT
7	NC 51	Idlewild Rd	South of David Dr	West side	0.57	35	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
9	NC 51	North side of Mint Lake Village Townhomes	North of David Dr	East side	0.36	35	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
31	Lebanon Rd	Margaret Wallace Rd	Barnsdale Ln	West side	0.68	35	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
36	Wilson Grove Rd	Lawyers Rd	Flaxton Dr	East side	0.43	35	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
37	Wilson Grove Rd	Flaxton Dr	Wilgrove Mint Hill Rd	East side	1.22	35	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
121	Brickstone Dr	Lawyers Rd	Northeast Middle School	West side	0.61	35	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
122	Brickstone Dr	Lawyers Rd	Northeast Middle School	East side	0.60	35	5-ft sidewalk	Mid-Term	L/S	MH/NCDOT
1	Idlewild Rd	Margaret Wallace Rd	Idlewild Road Park	West side	0.71	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
2	Idlewild Rd	Margaret Wallace Rd	North side of Mint Hill Middle School	East side	1.57	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
25	Margaret Wallace Rd	Idlewild Rd	Lebanon Rd	South side	0.99	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
55	Wilgrove Mint Hill Rd	Wilson Grove Rd	Jim Harper Ln	West side	1.36	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
57	Wilgrove Mint Hill Rd	Jim Harper Ln	Happy Hollow Dr	East side	0.26	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
64	NC 218	East of Jefferson Colony Rd	I-485	North side	0.19	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
65	NC 218	I-485	West of Plantation Falls Ln	North side	0.90	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
66	NC 218	I-485	Plantation Falls Ln	South side	1.08	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
106	Truelight Church Rd	North of Amadeus Dr	Wilgrove Mint Hill Rd	Southeast side	0.34	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
107	Truelight Church Rd	Wilgrove Mint Hill Rd	Tompkins Rd	Northwest side	0.49	30	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
4	Idlewild Rd	South side of Morning Star Lutheran Church	NC 41	North side	0.49	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
6	Idlewild Rd	NC 51	Thompson Rd	South side	0.59	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
8	NC 51	Idlewild Rd	North of Idlewild Rd	East side	0.03	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
22	NC 51	I-485	Albemarle Rd	West side	1.52	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
24	Margaret Wallace Rd	Idlewild Rd	Oak Dr	North side	0.14	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
26	Margaret Wallace Rd	Stoney Glen Dr	East of Gates Dr	North side	0.30	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
104	Truelight Church Rd	Versage Dr	Wilgrove Mint Hill Rd	Northwest side	0.64	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
112	Truelight Church Rd	North of Crown Hill Dr	NC 51	South side	0.82	26	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
69	Brief Rd	NC 218	Arlington Church Rd	North side	1.20	22	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
70	Brief Rd	NC 218	Arlington Church Rd	South side	1.18	22	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
75	Thompson Rd	Idlewild Rd	Fairington Oaks Dr	Northwest side	1.08	22	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
77	Thompson Rd	North of Fairington Oaks Dr	Ruth Haven Dr	North side	0.19	22	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
78	Thompson Rd	East of Castleford Dr	South Joli Cheval Ln	Northwest side	0.53	22	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
79	Thompson Rd	North of Joli Cheval Ln	Lawyers Rd	West side	0.45	22	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
56	Wilgrove Mint Hill Rd	Wilson Grove Rd	Jim Harper Ln	East side	1.37	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
68	NC 218	West of Olde Sycamore Dr	Mecklenburg / Union County line	North side	0.73	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
85	Bain School Rd	Gait Dr	Lawyers Rd	West side	0.77	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
86	Bain School Rd	Gait Dr	Lawyers Rd	East side	0.82	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
87	Bartlett Rd	NC 218	Stonebridge Way	Southeast side	0.67	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
88	Bartlett Rd	NC 218	Stonebridge Way	Northwest side	0.70	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT

Agency/Department	Funding Sources	
MH - Mint Hill	L - Local	N - National
MUMPO - Mecklenburg-Union Metropolitan Planning	General Fund	Transportation Enhancement Program
NCDOT - North Carolina Department of Transportation	Road Improvement Funds	CMAQ
CTT - Carolina Thread Trail	Powell Bill Funds	P - Private
		Carolina Thread Trail
	S - State	Developers
	Safe Routes to School	
	Governor’s Highway Safety Program	
	North Carolina Parks and Recreation Trust Fund	
	North Carolina Conservation Tax Credit Program	
	North Carolina Trails Program	



Sidewalk Projects										
Project ID	Project Location	From	To	Side of Road	Length (mi)	Rating Score	Improvements Needed	Scheduled Priority	Funding Source	Responsibility
88	Bartlett Rd	NC 218	Stonebridge Way	Northwest side	0.70	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
108	Truelight Church Rd	Wilgrove Mint Hill Rd	Tompkins Rd	Southeast side	0.48	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
109	Truelight Church Rd	Tompkins Rd	North of Crown Hill Dr	Southeast side	0.72	17	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
5	Idlewild Rd	NC 51	Thompson Rd	North side	0.61	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
91	Bartlett Rd	Lemington Dr	Arlington Church Rd	West side	0.85	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
92	Bartlett Rd	Lemington Dr	Arlington Church Rd	East side	0.83	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
94	Arlington Church Rd	Arlington Hills Dr	Bartlett Rd	West side	1.06	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
96	Arlington Church Rd	Arlington Hills Dr	Bartlett Rd	East side	1.08	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
97	Arlington Church Rd	Bartlett Rd	Hough Rd	West side	0.48	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
98	Arlington Church Rd	Bartlett Rd	Hough Rd	East side	0.48	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
99	Arlington Church Rd	Hough Rd	Cabarrus Rd	West side	0.65	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
100	Arlington Church Rd	Hough Rd	Cabarrus Rd	East side	0.63	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
110	Truelight Church Rd	North of Crown Hill Dr	North of Wexford Dr	West side	0.29	13	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
71	Brief Rd	Arlington Church Rd	Indian Trail Rd	North side	1.57	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
72	Brief Rd	Arlington Church Rd	Indian Trail Rd	South side	1.60	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
76	Thompson Rd	Idlewild Rd	Fairington Oaks Dr	Southeast side	1.10	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
80	Thompson Rd	Fairington Oaks Dr	Joli Cheval Ln	Southeast side	1.00	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
81	Thompson Rd	Joli Cheval Ln	Lawyers Rd	East side	0.55	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
93	Arlington Church Rd	Brief Rd	Arlington Hills Dr	West side	0.26	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
95	Arlington Church Rd	Brief Rd	Arlington Hills Dr	East side	0.25	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
116	Cabarrus Rd	Arlington Church Rd	Herbert Flowe Rd	South side	0.80	9	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
47	Lawyers Rd	Shelbourne Pl	Bain School Rd	Northeast side	0.79	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
48	Lawyers Rd	Shelbourne Pl	Bain School Rd	Southwest side	0.82	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
49	Lawyers Rd	Bain School Rd	I-485	Northeast side	0.91	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
50	Lawyers Rd	Bain School Rd	I-485	Southwest side	0.94	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
67	NC 218	Plantation Falls Ln	Mecklenburg / Union County line	South side	0.90	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
89	Bartlett Rd	Stonebridge Way	Lemington Dr	East side	0.56	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
90	Bartlett Rd	Stonebridge Way	Lemington Dr	West side	0.53	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
101	Arlington Church Rd	Cabarrus Rd	Albemarle Rd	East side	0.62	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
102	Arlington Church Rd	Cabarrus Rd	Albemarle Rd	West side	0.58	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
113	Cabarrus Rd	Albemarle Rd	Arlington Church Rd	North side	0.79	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
114	Cabarrus Rd	Albemarle Rd	Arlington Church Rd	South side	0.84	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
115	Cabarrus Rd	Arlington Church Rd	Herbert Flowe Rd	North side	0.81	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
117	Cabarrus Rd	Herbert Flowe Rd	Ferguson Rd	North side	1.07	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
118	Cabarrus Rd	Herbert Flowe Rd	Ferguson Rd	South side	1.07	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
119	Cabarrus Rd	Ferguson Rd	Mecklenburg / Cabarrus County line	North side	0.70	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
120	Cabarrus Rd	Ferguson Rd	Mecklenburg / Cabarrus County line	South side	0.72	4	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
53	Wilgrove Mint Hill Rd	Albemarle Rd	Wilson Grove Rd	West side	0.20	0	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
54	Wilgrove Mint Hill Rd	Albemarle Rd	Wilson Grove Rd	East side	0.20	0	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
73	Brief Rd	Indian Trail Rd	Mecklenburg / Union County line	North side	0.88	0	5-ft sidewalk	Long-Term	L/S	MH/NCDOT
74	Brief Rd	Indian Trail Rd	Mecklenburg / Union County line	South side	0.87	0	5-ft sidewalk	Long-Term	L/S	MH/NCDOT

Agency/Department	Funding Sources	
MH - Mint Hill	L - Local	N - National
MUMPO - Mecklenburg-Union Metropolitan	General Fund	Transportation Enhancement Program
NCDOT - North Carolina Department of	Road Improvement Funds	CMAQ
CTT - Carolina Thread Trail	Powell Bill Funds	P - Private
		Carolina Thread Trail
	S - State	Developers
	Safe Routes to School	
	Governor's Highway Safety Program	
	North Carolina Parks and Recreation Trust Fund	
	North Carolina Conservation Tax Credit Program	
	North Carolina Trails Program	

Greenway Projects									
Project ID	From	To	Local Plan	Length (mi)	Rating Score	Improvements Needed	Scheduled Priority	Funding Source	Responsibility
1	Idlewild Rd	NC 51 / Blair Rd	Mecklenburg County Greenways	3.53	83	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Short-Term	L	MCPR
15	SE of NC 51 (downtown overlay)	North of I-485	Comprehensive Land Use Plan	1.99	74	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Short-Term	L	MH/MCPR
5	Proposed greenway (west of Lawyers Rd)	Chicopee Dr	Comprehensive Transportation Plan	0.63	70	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Short-Term	L	MH/MCPR
16	NC 51 / Lawyers Rd	Lawyers Rd / Heath Lake Dr	Comprehensive Land Use Plan	1.31	70	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Short-Term	L	MH/MCPR
14	NC 51 / Windy Knoll Ln	N of NC 218/NC 51 Intersection	Comprehensive Land Use Plan	1.83	57	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Mid-Term	L	MH/MCPR
8	Proposed greenway (west of Idlewild Rd)	NC 51 / Matthews-Mint Hill Rd	Comprehensive Transportation Plan	1.87	48	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Mid-Term	L	MH/MCPR
7	South of Jim Harper Ln	I-485 / NC 51 Interchange	Comprehensive Land Use Plan	1.68	30	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
9	NC 51 / Matthews-Mint Hill Rd	Lawyers Rd	Comprehensive Transportation Plan	3.32	30	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
12	Proposed Greenway (east of Lebanon Rd)	Proposed Greenway / Well Rd crossing	Comprehensive Transportation Plan	2.20	26	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
2	NC 51 / Blair Rd	West of Bartlett Rd	Comprehensive Transportation Plan	1.86	22	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
10	Lawyers Rd	Quail Park Dr	Comprehensive Transportation Plan	1.64	22	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
17	Lawyers Rd / Heath Lake Dr	Proposed Greenway (south of Lawyers Rd)	Comprehensive Transportation Plan	1.10	22	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
19	NC 218 (east of Brief Rd)	Brief Rd / Arlington Church Rd	Comprehensive Land Use Plan	1.32	22	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
6	Chicopee Dr	South of Jim Harper Ln	Mecklenburg County Greenways	0.21	17	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
11	Mint Lake Dr	Proposed Greenway (just NW of Thompson Rd)	Comprehensive Land Use Plan	1.26	17	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
3	West of Bartlett Rd	Mecklenburg / Cabarrus County line	Comprehensive Land Use Plan	3.86	13	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
4	Proposed greenway (west of Bartlett Rd)	South of Cabarrus Rd	Comprehensive Land Use Plan	1.48	13	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
13	Proposed Mecklenburg Greenway (E of Beaver Dam Ln)	South of Davis Rd	Comprehensive Land Use Plan	1.48	13	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
21	Keltic Cove Dr	Northeast Middle School	Proposed Connector (Pedestrian Plan)	0.06	13	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
20	Proposed Greenway	Mint Hill Sports Complex	Comprehensive Land Use Plan	0.45	9	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR
18	North of Lawyers Rd / Bain School Rd	Proposed Greenway (E of Lawyers Rd/I-485)	Comprehensive Transportation Plan	0.57	4	10' Asphalt trail, regulatory/guiding signs, trash receptacles, benches, intersection warning signs	Long-Term	L	MH/MCPR

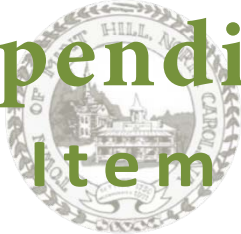
Agency/Department	Funding Sources	
MH - Mint Hill	L - Local	N - National
MUMPO - Mecklenburg-Union Metropolitan Planning	General Fund	Transportation Enhancement Program
NCDOT - North Carolina Department of Transportation	Road Improvement Funds	CMAQ
CTT - Carolina Thread Trail	Powell Bill Funds	P - Private
MCPR - Mecklenburg County Parks & Recreation		Carolina Thread Trail
	S - State	Developers
	Safe Routes to School	
	Governor’s Highway Safety Program	
	North Carolina Parks and Recreation Trust Fund	
	North Carolina Conservation Tax Credit Program	
	North Carolina Trails Program	

Intersection Projects					
Project Location	Intersection Type	Improvements Needed	Scheduled Priority	Funding Source	Responsibility
NC 51 / NC 218	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Short-Term	L/P	MH/NCDOT
NC 51 / Lawyers Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Mid-Term	L/P	MH/NCDOT
NC 51 / Idlewild Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Lawyers Road / Lebanon Road / Wilson Grove Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Lawyers Road / Bain School Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Brief Road / Arlington Church Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Cabarrus Road / Arlington Church Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Idlewild Road / Margaret Wallace Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Lebanon Road / Margaret Wallace Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Wilgrove-Mint Hill Road / Wilson Grove Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Wilgrove-Mint Hill Road / Albemarle Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
NC 51 / Lebanon Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
I-485 / Idlewild Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
I-485 / Albemarle Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
NC 51 / Albemarle Road	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
Albemarle Road / Clear Creek Commerce Drive	Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Long-Term	L/P	MH/NCDOT
NC 51 / Bain School Road	Non-Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Short-Term	L/P	MH/NCDOT
NC 51 / Phyliss Lane	Non-Signalized	Striped Crosswalks, Pedestrian Signals, Curb Ramps, Sidewalks, Pedestrian Lighting	Mid-Term	L/S	MH/NCDOT

Agency/Department	Funding Sources	
MH - Mint Hill MUMPO - Mecklenburg-Union Metropolitan Planning Organization NCDOT - North Carolina Department of Transportation CTT - Carolina Thread Trail	L - Local	N - National
	General Fund	Transportation Enhancement Program
	Road Improvement Funds	CMAQ
	Powell Bill Funds	P - Private
		Carolina Thread Trail
	S - State	Developers
	Safe Routes to School	
	Governor’s Highway Safety Program	
	North Carolina Parks and Recreation Trust Fund	
	North Carolina Conservation Tax Credit Program	
	North Carolina Trails Program	



**MINT HILL PEDESTRIAN MASTER PLAN**



# Appendix C

## Action Item Matrix



## MINT HILL PEDESTRIAN MASTER PLAN



Action Items		Lead	Support	Scheduled Priority	Status
Planning					
P.1	Present Plan Recommendations to Planning Board for Endorsement	P&ZP	C	Immediate	In-Progress
P.2	Present Plan recommendations to Board of Commissioners for adoption	P&ZP	C	Immediate	In-Progress
P.3	Adopt the Pedestrian Master Plan	BOC	PB	Immediate	In-Progress
P.4	Incorporate Standards and Guidelines (Chapter 5) into the Town's Development Standards	P&ZP	BOC	Immediate	
P.7	Establish a Transportation Committee (TC)	P&ZP	PW	Immediate	
P.8	Establish a tool (online or hot line) to allow pedestrian to report issues and/or concerns	P&ZP	PW	Short-Term	
P.9	Continually assess and evaluate the effectiveness of current pedestrian related policies and improve when necessary.	P&ZP	TC	On-Going	
P.10	Continue to review land development plans for the inclusion of pedestrian facilities (i.e. sidewalks and greenways/trails)	P&ZP	PW	On-Going	
P.11	Continually assess and evaluate the progress of the Pedestrian Master Plan	P&ZP	TC	On-Going	
Funding					
F.1	Pursue Safe Routes to School grants for programs, policies and infrastructure projects	P&ZP	NCDOT	Immediate	
F.2	Develop a long-term funding strategy for the development of pedestrian facilities	P&ZP	BOC	Immediate	
F.3	Pursue additional grants to implement pedestrian programs and projects	P&ZP	TC	On-Going	
F.4	Purchase land for the construction of greenways	P&ZP	CTT	On-Going	
Development/Construction					
D.1	Complete sidewalks within the Downtown Area along NC 51	PW	NCDOT	Immediate	
D.2	Build proposed greenways	PW	CTT	Long-Term	
D.3	Include pedestrian facilities into the construction of new and reconstructed roadways	PW	NCDOT	On-Going	
Maintenance					
M.1	Incorporate pedestrian maintenance into the Town's maintenance program	PW	P&ZP	Immediate	
M.2	Develop a system for users to be able to call in maintenance concerns	PW	P&ZP	Short-Term	
Education					
E.1	Develop communication program to educate the community about the Pedestrian Master Plan	P&ZP	TC	Immediate	
E.2	Work with local media to raise pedestrian safety awareness	P&ZP	TC	Short-Term	
E.3	Create and update an Mint Hill Pedestrian System Map with safety information and pedestrian laws	P&ZP	NCDOT	Mid-Term	
E.4	Continue and expand upon existing pedestrian awareness events	P&ZP	TC	On-Going	
E.5	Continue to coordinate with Carolina Thread Trail on the development of regional greenway connections	P&ZP	CTT	On-Going	
Coordination					
C.1	Initiate communication with surrounding municipalities to discuss pedestrian related issues and to coordinate on adjacent pedestrian projects	P&ZP	TC	Immediate	
C.2	Initiate discussions with local and regional health organizations to educate community about benefits of walking	P&ZP	TC	Immediate	
C.3	Coordinate with NCDOT and the Town's Public Works Department to ensure pedestrian facilities are incorporated into all roadway projects	P&ZP	PW	On-Going	
C.4	Coordinate with Charlotte Area Transportation System (CATS) to ensure the inclusion of benches, shelters, lighting and trash receptacles in any future transit improvement projects	P&ZP	CATS	On-Going	
C.5	Work with CMS to improve pedestrian connectivity to schools	P&ZP	CMS	On-Going	
C.6	Continue to coordinate with CTT on the development of greenway corridors	P&ZP	CTT	On-Going	

**Legend**

**Scheduled Priority**

I - Immediate (1-3 years)

S - Short-Term (3-5 years)

M - Mid-Term (5-10 years)

L- Long-Term (over 10 years)

O - On-Going

**Agency/Department**

P&ZD - Neighborhood & Zoning Department

BOC - Board of Commissioners

PB - Planning Board

PW - Public Works

D - Developers

TC - Transportation Committee

LR&AG - Local Residents & Advocacy Groups

PD - Police Department

MUMPO - Mecklenburg-Union Metropolitan Planning Organization

NCDOT - North Carolina Department of Transportation

CATS - Charlotte Area Transportation System

CTT - Carolina Thread Trail

CMS - Charlotte-Mecklenburg Schools

C - Consultant



**MINT HILL PEDESTRIAN MASTER PLAN**



# Appendix D

## Terminology



## **MINT HILL PEDESTRIAN MASTER PLAN**

## Terminology

**AASHTO:** Is the American Association of State Highway and Transportation Officials, which is a nonprofit, nonpartisan association representing highway and transportation departments of all transportation modes.

**ADA:** Refers to the American Disabilities Act of 1991 which gives civil rights protections to individuals with disabilities including equal opportunities in public accommodations, employment, transportation, state and local government services, and telecommunications.

**Charlotte Area Transit System (CATS):** Maintains a dual focus, managing day-to-day operations of Charlotte's transit services while planning for a regional transit system which will include bus rapid transit, light rail, commuter rail, and expanded bus service within a six-county area.

**Connectivity:** The logical and physical interconnection of functionally related points so that people can move among them

**Crosswalk:** A designated point on a road at which some means are employed to assist pedestrians who wish to cross a roadway or intersection. They are designed to keep pedestrians together where they can be seen by motorists, and where they can cross most safely with the flow of vehicular traffic.

**Curb Extension:** A section of sidewalk at an intersection or mid-block crossing that reduces the crossing width for pedestrians and is intended to slow the speed of traffic and increase driver awareness

**FHWA:** Stands for the Federal Highway Administration who carries out the Federal highway programs in partnership with the State and local agencies to meet the Nation's transportation needs.

**Greenway/Trail:** Is a linear path or open space, often composed of natural vegetation. Greenways can be used to create connected networks of open space that include traditional parks and natural areas specifically designed for pedestrian and bicycle use. Greenways provide an off-street component to the bicycle network.

**Intersection:** Is where two or more pathways or roadways join together

**LRTP:** Stands for the Long Range Transportation Plan that is managed by the Mecklenburg-Union Metropolitan Planning Organization (MPO) and is a federally mandated, long-term planning document detailing the transportation improvements and policies to be implemented in the MPO's planning area.

**Median:** Is considered to be a physical barrier that is constructed of concrete, asphalt, or landscaping, that separates two directions of traffic

**Multi Use Path:** Is a paved path (concrete or asphalt), typically 10- feet wide, physically separated from motorized vehicular traffic by an open space or landscaped barrier and located within the highway right-of-way.

**MUMPO:** Mecklenburg-Union Metropolitan Planning Organization

**MUTCD:** Is the Manual of Uniform Traffic Control Devices, which is the national standards guidebook on signage and pavement marking for roadways

**NCDOT:** North Carolina Department of Transportation

**Pedestrian:** Is a person that is traveling by foot or a person on roller skates, roller blades, child's tricycle, non-motorized wheelchair, skateboard, or other non-powered vehicles (excluding bicycles)

**Quality of Life:** Is a measure of the standard of living which considers non-financial factors such as health, functional status and social opportunities that are influenced by disease, injury, treatment or social and political policy

**ROW (right of way):** Is an easement held by the local jurisdiction over land owned by the adjacent property owners that allows the jurisdiction to exercise control over the surface and above and below the ground of the right-of-way; usually designated for passage

**Safe Routes to School:** Is a federal program that provides funding to encourage and facilitate the planning and implementation of bicycle and pedestrian projects near schools.

**Sidewalk:** Is typically a concrete facility that is located in the public right-of-way adjacent to a roadway. The facility can also be asphalt, brick or other materials.



**Thoroughfare:** Is a road that leads at either end to another street and

**Transportation Committee:** Is a group of volunteers who work with the Town to provide direction and guidance to the Planning and Zoning and Public Works Departments in its efforts to make the Town of Mint Hill pedestrian friendly.



## MINT HILL PEDESTRIAN MASTER PLAN



# Appendix E

## Funding



## MINT HILL PEDESTRIAN MASTER PLAN

## 1.1 Overview

The intent of this appendix is to identify various funding opportunities that the Town of Mint Hill can use for the planning, design and construction of pedestrian improvement projects as well as educational programs. Perhaps one of the most important recommendations and future funding sources is a dedicated local funding source that the Town can create for the development of pedestrian and greenway improvements.

## 1.2 Funding Opportunities

There are several opportunities that the Town of Mint Hill can capitalize on to help pay for the pedestrian improvements outlined in this report. The following provides a summary of the various local, state and federal funding sources that are available for the Town to use. The list below represents many of the core funding strategies that are available, but is by no means an exhaustive list of funding sources. There are many other sources available that should be researched and pursued as well.

### 1.2.1 Local Funding

There are several local funding opportunities that the Town can use to implement the recommendations in this Pedestrian Master Plan. The following list of funding sources provides a brief explanation of these funds and what they can be used for.

#### Local General Funds

The Town currently dedicates \$50,000 from the local general fund that goes toward the sidewalk program. Approximately \$10,000 of that \$50,000 is dedicated for the maintenance of existing sidewalks and the remaining \$40,000 is for the purchase of right-of-way and/or the construction of new sidewalks.

#### Road Improvement Funds

The Town currently utilizes these funds to maintain approximately 100 miles of roadways within the Town's jurisdictional limits. These funds cannot be used to construct sidewalks because all of the money is currently used to maintain the existing roadways.

### Powell Bill Fund

The Town currently utilizes these funds to maintain approximately 100 miles of roadways within the Town's jurisdictional limits. Although the Powell Bill Fund can be used for new sidewalk construction and maintenance, these funds have generally been used exclusively on the maintenance of roadways.

### **1.2.2 State Funding**

There are several state funding sources that can be used to implement the Pedestrian Master Plan. Many of the state funding sources are actually funded by the federal government, but are administered through the state agencies. The North Carolina Department of Transportation Division of Bicycle and Pedestrian Transportation has been funding pedestrian related infrastructure and non-infrastructure projects.

### Safe Routes to Schools Program

Safe Routes to School (SRTS) is a program that enables and encourages children to walk and bike to school. The program helps make walking and bicycling to school a safe and more appealing transportation option for children. It facilitates the planning, development, and implementation of projects and activities that will improve safety and reduce traffic, fuel consumption, and air pollution in the vicinity of school. The Safe Routes to Schools Program is funded through SAFETEA-LU and currently the State of North Carolina has over \$4 million of total funding for this program. The following provides some information about the program and how it can be used to improve pedestrian safety in Mint Hill.

Different types of grants are available through this program; these include **Action Plans, Non-Infrastructure Grant Reimbursement, Infrastructure Grant Reimbursement, and Highway Division Funds.**

- **Action Plans:** are awarded in the amount of \$15,000-\$30,000 to develop plans to improve pedestrian and bicycle safety within a two-mile radius of schools that are grades K-8. The State has a total of \$300,000 in this program to award to communities around the state.
- **Non-Infrastructure Grants:** are awarded in the amount of \$10,000-\$50,000 and can be used for pedestrian and bicycle education, encouragement, and enforcement. These grants are good for developing programs that encourage children to walk and bike to school.



- **Infrastructure Grants:** are funds that are awarded for the planning, design, and construction of pedestrian and bicycling facilities within a 2-mile radius of a school. Funding requests may range from \$100,000 to \$300,000 per project. The total doesn't have to be spent on one project. Any agency that is willing and able to enter into a reimbursement agreement with NCDOT and has the authority to construct and/or install and maintain infrastructure is eligible to apply. Types of projects that are eligible may include sidewalk improvements, crossing improvements, on-street bike and pedestrian improvements, bike parking, traffic calming, and traffic separation devices among others. An adopted Action Plan that identifies needed infrastructure improvements is helpful in obtaining these grants.
- **Highway Division Funds:** are funds that are allocated to each of NCDOT's 14 Highway Divisions to fund infrastructure projects on state-maintained roadways. The projects must be within 2-miles of a school serving grades K-8 to be eligible. The grants range from \$10,000-\$50,000 and can be used to improve conditions for walking and biking to school. Typically these grants are used for the following: sidewalks, traffic-calming, on-road bicycling and walking may be included in the Transportation Improvement Program (TIP) as part of the construction of a highway project. The Town coordinates with the Mecklenburg-Union Metropolitan Planning Organization (MUMPO). Transportation improvement projects can include bicycle and pedestrian projects. These projects are then placed on a list that contains projects from other municipalities in order to compete for potential funding. The timeline for the current STIP is 2009-2015. MUMPO is currently in the process of developing its 2012-2018 TIP in conjunction with NCDOT.

Pedestrian related projects are eligible for funding from this program as independent projects, such as greenways, which are separate from a roadway construction or widening project. There are two categories for funding of bicycle and pedestrian projects.

- **Independent Projects:** are projects that occur independently of scheduled highway projects.
- **Incidental Projects:** are projects that occur as part of a scheduled highway improvement project.

#### Governor's Highway Safety Program (GHSP)

The GHSP provides funds for pedestrian and bicycle related initiatives upon approval. This is an annual program and the amounts of the funds vary from year to year, according to the specific amounts requested.

#### North Carolina Parks and Recreation Trust Fund (PARTF)

In 1994 the North Carolina General Assembly established the Parks and Recreation Trust Fund (PARTF). PARTF was established to fund improvements in the state's park system, to fund grants for local governments and to increase the public's access to the state's beaches. PARTF funds are used to acquire, build, and renovate parks. They provide a dollar-for-dollar match up to \$500,000. The Town should apply for this grant money to build greenways and other recreational facilities that serve the general public. To find out more information about the PARTF program, please visit the following web page [http://www.ncparks.gov/About/grants/partf\\_main.php](http://www.ncparks.gov/About/grants/partf_main.php).

#### The North Carolina Conservation Tax Credit Program

North Carolina recognizes the importance of land conservation to its economy, and offers a tax credit program to promote conservation of ecosystem functions (fish and wildlife conservation and conservation of natural areas), ecosystem services (farmland conservation) and other public benefits (public access to public trails, waters, and beaches). This program is managed by the North Carolina Department of Environment and Natural Resources, provides an incentive (in the form of an income tax credit) for landowners that donate interests in real property for conservation purposes. Property donations can be fee simple or in the form of conservation easements or bargain sale. More information on this program can be obtained at the following web page:

<http://www.onencnaturally.org/pages/ConservationTaxCredit.html>

#### North Carolina Trails Program

This program is administered through the North Carolina Division of Parks and Recreation. The program originated in 1973 and is dedicated to helping citizens, organizations and agencies plan, develop and manage all types of trails. The North Carolina Division of Parks and Recreation offers two types of grants. The first is the Adopt-A-Trail Grant and the other is the Recreational Trails Program Grant. To find out more information about this program, please visit their web page at [http://www.ncparks.gov/About/trails\\_main.php](http://www.ncparks.gov/About/trails_main.php).

### **1.2.3 National Funding Sources**

There are a wide range of national funding opportunities that can be used for the development of bicycle facilities. The following list represents the most widely referenced and used.

#### **Transportation Enhancement Program**

The Transportation Enhancements Program, which is administered through the FHWA, provides funding for the implementation of bicycle and pedestrian facilities, landscaping and aesthetic improvements. Eligible pedestrian improvement projects include:

- Construction of new sidewalks and separate walking trails/paths

In addition to physical improvement projects, there are safety programs and materials that are allowable. These include:

- Pedestrian safety training
- Related training material such as brochures

Additional information can be obtained at the NCDOT Enhancement Program web page at: <http://www.ncdot.gov/programs/Enhancement/>

#### **Congestion Mitigation & Air Quality Improvement Program**

The Congestion Mitigation and Air Quality Improvement Program (CMAQ) provides \$6 billion in funding for surface transportation and other related projects that contribute to air quality improvements and reduce congestion. The Town has actively pursued and received CMAQ funding and should continue to do so in the future. To find more information regarding CMAQ funding, please use the following link:

[http://www.fhwa.dot.gov/environment/air\\_quality/cmaq/index.cfm](http://www.fhwa.dot.gov/environment/air_quality/cmaq/index.cfm)



# MINT HILL PEDESTRIAN MASTER PLAN